

Staff Report City of Loma Linda

From the Department of Community Development

PLANNING COMMISSION MEETING OF MAY 2, 2012

TO: PLANNING COMMISSION

FROM: KONRAD BOLOWICH, ASSISTANT CITY MANAGER,
COMMUNITY DEVELOPMENT DEPARTMENT

SUBJECT: PRECISE PLAN OF DESIGN (PPD) NO. 11-105

SUMMARY

The applicant requests to construct a three-story, 8,921 square-foot motel inn and suites. The proposed project would include the construction of 23 rooms and the associated on-site improvements. The project site is located on the north side of Redlands Boulevard in the East Valley Corridor Specific Plan/General Commercial (EV/CG) Zone).

RECOMMENDATION

The recommendation is for the Planning Commission to:

1. Adopt the Mitigated Negative Declaration (Attachment B); and,
2. Approve PPD No. 11-105 based on the Findings, and subject to the Conditions of Approval (Attachment C).

PERTINENT DATA

Property Owner/Applicant:	Hitesh Patel
General Plan/Zoning:	Commercial/East Valley Corridor Specific Plan, General Commercial (EV/CG)
Site:	Vacant 0.41 acre rectangular lot
Topography:	Generally flat
Vegetation:	None
Special Features:	N/A

EXHIBIT - A

BACKGROUND AND EXISTING SETTING

Background

On August 20, 2008, the Planning Commission approved Precise Plan of Design PP 07-04 to develop an 8,921 square foot, 3-story motel at the subject site. Due to personal issues, the applicant was unable to continue with the project and project approval expired on August 20, 2009.

This project is essentially the same as the one unanimously approved by the Planning Commission in 2008 (PPD 07-04).

Existing Setting

The 18,000 square-foot site is vacant and devoid of any structures. The site is bounded by the Interstate 10 Freeway to the north, Redlands Boulevard to the south, an existing fast food, drive-thru restaurant to the east, and a vacant, legal non-conforming structure to the west. The site is in close proximity to the commercial centers, located at the Redlands Boulevard/Anderson Street intersection, that accommodate a variety of retail and service oriented businesses.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) STATUS

Pursuant to CEQA, the City proposes to adopt a Mitigated Negative Declaration for the project. Staff has found that the project will not have a significant effect on the environment on the basis of the Initial Study with mitigation measures in place. A Notice of Intent (NOI) to Adopt a Mitigated Negative Declaration was prepared and issued on April 13, 2012. The CEQA mandatory 20-day public review period began on Thursday, April 13, 2012 and ended on Wednesday, May 2, 2012. No comments were received as of the preparation of this report.

Public Comments

Public hearing/environmental review notices for this project were posted and mailed to property owners and occupants within 300 feet of the project site on April 13, 2012. As of this report, the City has received no written or oral comments on the proposal.

ANALYSIS

Project Description

The project is a request to construct a new, three-story 8,921 square-foot motel building on an existing 0.41 acre vacant lot. The building will be placed on the western portion of the parcel with the majority of the required parking located on the east side of the property. The site will be screened by mature trees and landscaping on the north and east boundaries of the parcel. Additionally, the building will maintain the required 25-foot front set back in landscaping. The footprint of the proposed building is adjacent to the west property line of the subject parcel.

Site Analysis

The project site will be completely graded for construction. The building foot print will cover just under 3,000 square feet (15 percent) of the graded area. The EV/CG zone requires a minimum 25 foot front-yard setback and no minimum setbacks on the side yards. The rear setback adjacent to the freeway right-of-way requires a minimum 25 feet of landscape area, however, provisions for a smaller landscape area is allowed with the incorporation of landscaping enhancements elsewhere on site. At its nearest point, the proposed building is 5 feet from the property line (west property line). The rear building setback will have a minimum dimension of 25 feet. The proposed building will maintain the required 25-foot set back from the front property line.

The site plan illustrates one vehicular ingress and egress to the proposed site. The point of access is located from Redlands Boulevard. Additionally, a pedestrian access will be provided from the City sidewalk to the south via a walkway to the first floor lobby located on the south side of the building. The second and third levels of the building will be accessed by an elevator, as well as interior stairways on the north and south sides of the building.

Pursuant to the East Valley Corridor Specific Plan, Section EV4.201 (Parking Requirements), the parking ratio for a motel is 1.1 spaces for every sleeping unit. According to the project data and the prescribed parking ratio, the project is required to have a minimum of 25 parking spaces. The proposed project provides 27 spaces (including one accessible space) which meet the City's parking requirement.

The site plan includes the following minor changes:

- The orientation of the trash enclosure was slightly modified, but is still located in the same general area;
- The handicapped accessible parking space was relocated;
- The southernmost landscape finger was relocated;

Please be aware that the landscape plan will be modified to reflect the changes to the site plan.

Architecture Analysis

The proposed building incorporates architectural elements such as gabled tile roofs and earth toned stucco walls that would blend with the other adjacent commercial structures in the area. The building is designed in a modern and contemporary architectural style with rectangular glazed windows, decorative exterior walkway railings, and the incorporation of vertical and horizontal score lines. The use of stone veneer accents further enhances the contemporary style. The total vertical height of the building is 31 feet, which is measured from the apex of the gabled roof.

Measure V Compliance

On November 7, 2006, the Loma Linda voters passed Measure V, *The Residential and Hillside Development Control Measure*. Staff analyzed the project using the adopted development guidelines in Chapter 19.16 of the Loma Linda Municipal Code (LLMC) and determined that the project complies with the requirements of Measure V, as follows:

Section I (F)(2) of Measure V requires that traffic levels of service (LOS) be maintained at level C or better.

Section I (F)(2) – *To assure the adequacy of various public services and to prevent degradation of the quality of life experienced by the residents of Loma Linda, all new development projects shall assure by implementation of appropriate mitigation measures that, at a minimum, traffic levels of service (LOS) are maintained at a minimum of LOS C throughout the City, except where the current level of service is lower than LOS C. In any location where the level of service is below LOS C at the time an application for a development project is submitted, mitigation measures shall be imposed on that development project to assure, at a minimum, that the level of traffic service is maintained at levels of service that are no worse than those existing at the time an application for development is filed. In any location where the Level of Service is LOS F at the time an application for a development project is submitted, mitigation measures shall be imposed on that development project to assure, at a minimum, that the volume to capacity ratio is maintained at a volume to capacity ratio that is no worse than that existing at the time an application for development is filed. Projects where sufficient mitigation to achieve the above stated objectives is infeasible shall not be approved unless and until the necessary mitigation measures are identified and implemented.*

The original traffic impact analysis was prepared by Clyde Sweet and Associates in 2008 and LSA conducted a peer review of the traffic impact analysis. In December 2011 in association with this application, Clyde Sweet and Associates updated the traffic analysis and concluded that the project will generate approximately 9 trips per hour during a.m. peak hours and 11 trips per hour during p.m. peak hours of traffic, no change from the previous traffic analysis. The study concluded that the low volume of trips generated by the proposed motel project is not enough to significantly change the existing traffic levels for the intersections of Redlands Boulevard/Anderson Street, Redlands Boulevard/Richardson Street, and Redlands Boulevard/Mountain View Avenue, and Redlands Boulevard and Poplar Street.

As outlined in Section I (A)(3) of Measure V, the project includes a condition that requires the applicant to pay all required development impact fees to cover 100 percent of the pro rata share of the estimated cost of public infrastructure, facilities, and services.

The building and site meet the requirements Section I (A), Section 1(C) Principal Three, which requires that new developments be planned and constructed in a manner that preserves natural scenic vistas and protects against intrusion on view shed areas. Please refer to the Architectural Analysis section of this report, which provides a description of the proposed motel building and the project plans in Attachment D. The architectural design is compatible with other commercial complexes near the project site. The height of the building does not exceed 35 feet and the massing is appropriate to the site. The building will preserve scenic vistas to the north and will not result in intrusions into the view shed of the South Hills.

The majority of other requirements outlined in Measure V are for residential projects and do not apply to non-residential uses.

Precise Plan of Design Findings

According to LLMC Section 17.30.290, Precise Plan of Design (PPD), Application Procedure, PPD applications shall be processed using the procedure for a variance (as outlined in LLMC Section 17.30.030 through 17.30.060) but excluding the grounds (or findings). As such, no specific findings are required. However, LLMC Section 17.30.280, states the following:

“If a PPD would substantially depreciate property values in the vicinity or would unreasonably interfere with the use or enjoyment of property in the vicinity by the occupants thereof for lawful purposes or would adversely affect the public peace, health, safety or general welfare to a degree greater than that generally permitted by this title, such plan shall be rejected or shall be so modified or conditioned before adoption as to remove the said objections.”

The project is consistent with the General Plan Land Use designation (Commercial) and in compliance with the East Valley Corridor Specific Plan/General Commercial Zone, which permits both motels and hotels. The proposed motel use is compatible with the existing and future land uses in the surrounding area.

The project will provide improvements in the form of a new 8,921 square-foot, three-story motel inn and suites with on-site improvements including parking and drive aisles, pedestrian walkways, and landscaping. Staff recommends approval of the project to facilitate the development of a commercial business. The project will not adversely affect the public peace, health, safety or general welfare of the community.

In an effort to ensure that the foregoing project is consistent with the General Plan, compliant with the zoning and other City requirements, compatible with the surrounding area, and appropriate for the site, staff and the City Attorney have opted to apply the Conditional Use Permit Findings in LLMC §17.30.210 to this project, as follows:”

1. *That the use applied for at the location set forth in the application is properly one for which a conditional use permit is authorized by this title.*

The proposed use is a permitted use within the East Valley Corridor Specific Plan/General Commercial Zone (EVCSP/CG). The proposed 8,921 square-foot three-story motel structure is compatible is use with the existing commercial uses near the site. The proposed project has been designed in accordance with the standards and requirements of the EV/CG zone and it is consistent with all provisions contained in the General Plan.

2. *That the said use is necessary or desirable for the development of the community, is in harmony with the various elements and objectives of the general plan, and is not detrimental to existing uses specifically permitted in the zone in which the proposed use is to be located.*

The project is consistent with General Plan (July 25, 2008) Guiding Policy 4.6.3, which encourages the protection of the fiscal and financial health of the City. As with any new development, the developer will be required to pay for its fair share of new infrastructure and facilities in order to ensure that no increase will occur to the cost of public services provided to existing development. In addition, the proposed motel will provide the City with revenue through transient occupancy taxes.

As indicated in the discussion of Measure V Compliance, the project is also consistent with the General Plan as amended by Measure V.

3. *That the site for the intended use is adequate in size and shape to accommodate said use and all of the yards, setbacks, walls, or fences, landscaping and other features required in order to adjust said use to those existing or permitted future uses on land in the neighborhood.*

The subject parcel is adequate in size and shape to accommodate the proposed use. The lot coverage of the new facility is approximately seventeen (17) percent of the overall project site. The project meets the development criteria prescribed for the EV/CG zone including setbacks, yards and landscaping. Therefore, the project site can accommodate the proposed use which will be compatible with the existing and future land uses along the Redlands Boulevard corridor.

4. *That the site or the proposed use related to streets and highways is properly designed and improved to carry the type and quantity of traffic generated or to be generated by the proposed use.*

The project site proposes a commercial driveway approach on Redlands Boulevard that will provide ingress/egress. Based on the Traffic Impact Analysis (TIA), project trip distributions (PCE's) were estimated to be at about 9 trips per hour during the a.m. peak hour and 12 trips during the p.m. peak hour. The TIA indicates that the project will not result in any significant impacts that would substantially increase either the number of vehicle trips or the volume to capacity ratio on roads.

5. *That the conditions set forth in the permit and shown on the approved site plan are deemed necessary to protect the public health, safety and general welfare.*

The public health, safety and general welfare will be protected with the implementation of the Conditions of Approval for this Precise Plan of Design. Specifically, Conditions number 56 through 59 require the developer to provide sufficient lighting for site and among other things, register with the Crime Free Hotel/Motel Program that works closely with the San Bernardino County Sheriffs Department personnel to address crime prevention.

CONCLUSION

Staff recommends approval of the project because it is consistent with the General Plan (as amended by Measure V) and in compliance with the LLMC Code and East Valley Corridor Specific Plan, General Commercial (EV/CG) requirements. The motel use is compatible with the existing and future uses in the surrounding area. The Draft NOI/Initial Study was prepared pursuant to CEQA and the CEQA Guidelines and mitigation measures have been incorporated into the project as Conditions of Approval. Finally, the findings have been made to support approval of the Precise Plan of Design request.

Report prepared by:

Guillermo Arreola
Associate Planner

EXHIBITS

- A. Vicinity Map
- B. Mitigated Negative Declaration (NOI/Initial Study)
- C. Conditions of Approval
- D. Project Plans

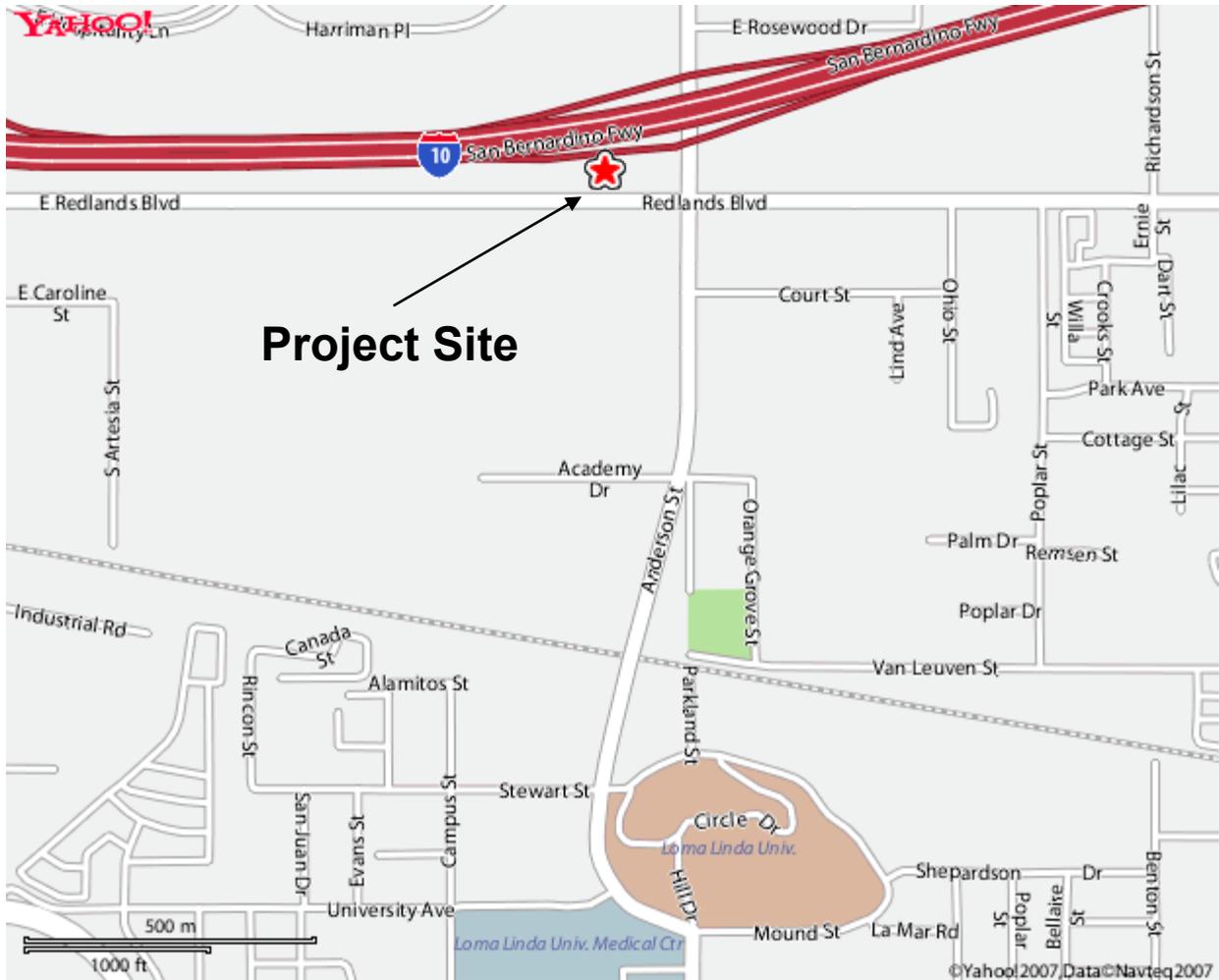
Blank Page



City of Loma Linda

Community Development Department

25541 Barton Road, Loma Linda, CA 92354
(909) 799-2830; Fax (909) 799-2894



Project Site



Vicinity Map

EXHIBIT - A

Blank page

CITY OF LOMA LINDA
NOTICE OF INTENT
TO ADOPT A MITIGATED NEGATIVE DECLARATION
OF ENVIRONMENTAL IMPACT

FROM: CITY OF LOMA LINDA
Community Development Department
25541 Barton Road
Loma Linda, CA 92354

TO: OFFICE OF PLANNING AND RESEARCH
1400 Tenth Street, Room 121
Sacramento, CA 95814

COUNTY CLERK
County of San Bernardino
385 North Arrowhead Avenue
San Bernardino, CA 92415

SUBJECT: Filing of Notice of Intent (NOI) to adopt a Mitigated Negative Declaration in compliance with Section 21080c of the Public Resources Code and Sections 15072 and 15073 of the CEQA Guidelines.

Project Title: **PRECISE PLAN OF DESIGN (PPD) NO. 11-105**

State Clearinghouse Number (if submitted to Clearinghouse):

Lead Agency Contact Person: Guillermo Arreola **Area Code/Telephone:** 909-799-2839

Project Location (include county): The project is located on the north side of Redlands Boulevard (APN: 0281-091-41) in the existing East Valley Corridor Specific Plan/General Commercial (EV/CG) zone within the City of Loma Linda and County of San Bernardino.

Project Description: The project is a request to construct a new 8,921 square-foot, three-story motel and suites. The proposed project would include the construction of 23 rooms and the associated on-site parking and landscaping improvements.

The project site is a general commercial use and is not listed in the California Hazardous Waste and Substances Site List (Cortese List) pursuant to Government Code Section 65962.5(E) for soil or ground water contamination.

This is to notify the public and interested parties of the City of Loma Linda's intent to adopt a Mitigated Negative Declaration for the above-referenced project. The mandatory public review period will begin on **Friday, April 13, 2012** and will end on **Wednesday, May 2, 2012**. The NOI/Initial Study is available for public review at the public counter in the Community Development Department, 25541 Barton Road, and the Loma Linda Library, 25581 Barton Road, east end of the Civic Center.

During the public review period, the project and proposed Mitigated Negative Declaration will be reviewed by the **Planning Commission** in a public hearing on **Wednesday, May 2, 2012**, at 7:00 p.m. in the Council Chambers located of the main lobby of City Hall (address listed above).

Signature: _____
Guillermo Arreola

Title: Associate Planner
Date: April 12, 2012

I:\PROJECT FILES\PPD's\2011\PPD 11-105 LL 23-Unit Motel\NOI Mitigated Neg Dec - Exhibit B.doc

Blank Page

CITY OF LOMA LINDA
ENVIRONMENTAL CHECKLIST FORM
AND INITIAL STUDY

Project Title: Precise Plan of Design (PPD) NO. 11-105

Lead Agency Name: City of Loma Linda Community Development Department
Address: 25541 Barton Road
Loma Linda, CA 92354

Contact Person: Guillermo Arreola
Phone Number: (909) 799-2839
Email: garreola@lomalinda-ca.gov

Project Sponsor: Hitesh Patel
Address: 9999 E. Jasmine Street
Scottsdale, AZ 85260

General Plan Designation: Commercial

Project Location (Address/Nearest cross-streets): The proposed project is located on a vacant 18,115 square-foot parcel (APN: 0281-091-41) in the City of Loma Linda. The site is on the north side of Redlands Boulevard west of Anderson Street (refer to Figure 1: Regional Location Map and Figure 2: Vicinity Map).

Project Description: The applicant/sponsor is proposing a new 8,921 square-foot, three-story motel inn and suites (Figure 3: Site Plan). The proposed project would include the construction of 23 rooms and the associated on-site parking and landscaping improvements. The proposed development would be constructed in an area that is mostly flat and void of vegetation.

Surrounding Land Uses and Setting (Briefly describe the project's surroundings): The approximate 0.42-acre site is adjacent to a freeway off ramp to the north, an existing commercial establishment to the south, a fast food restaurant to the east, and a legal non-conforming residential structure to the west. The project site is designated Commercial by the City's General Plan (July 25, 2006) and zoned as East Valley Corridor Specific Plan General Commercial (EV/CG).

Figure 1

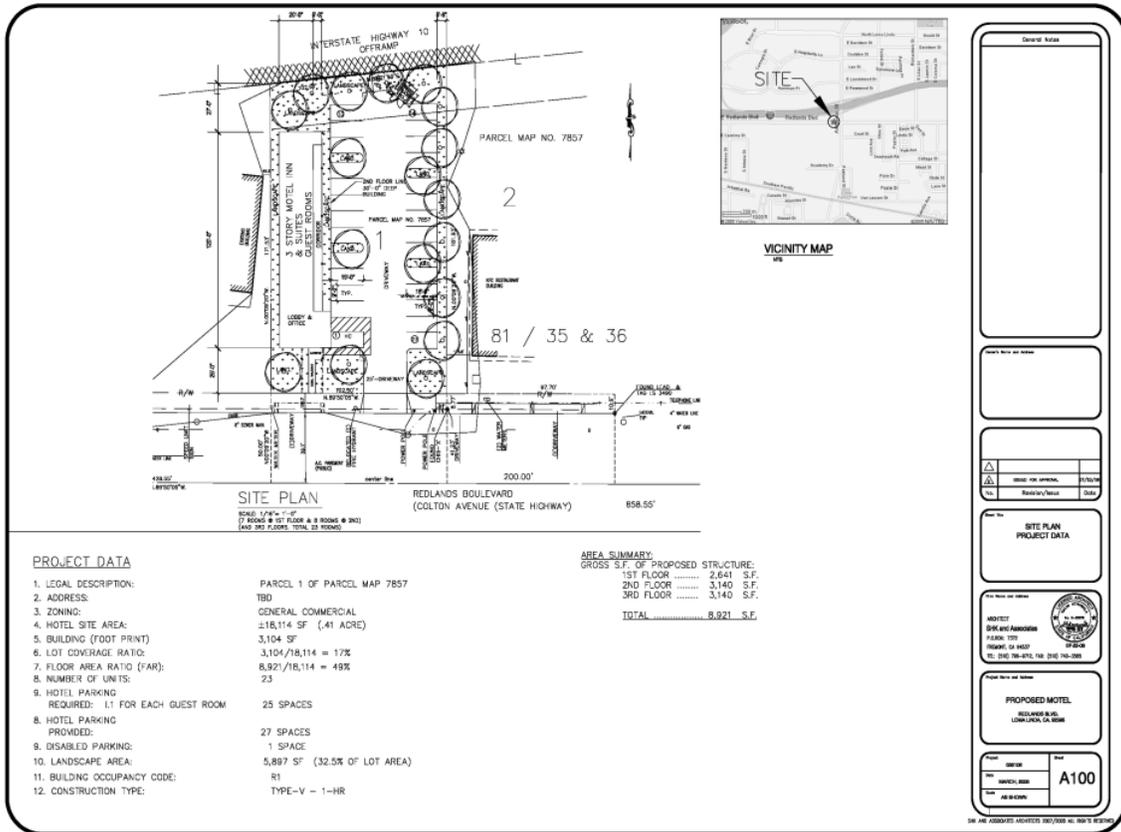


Blank Page

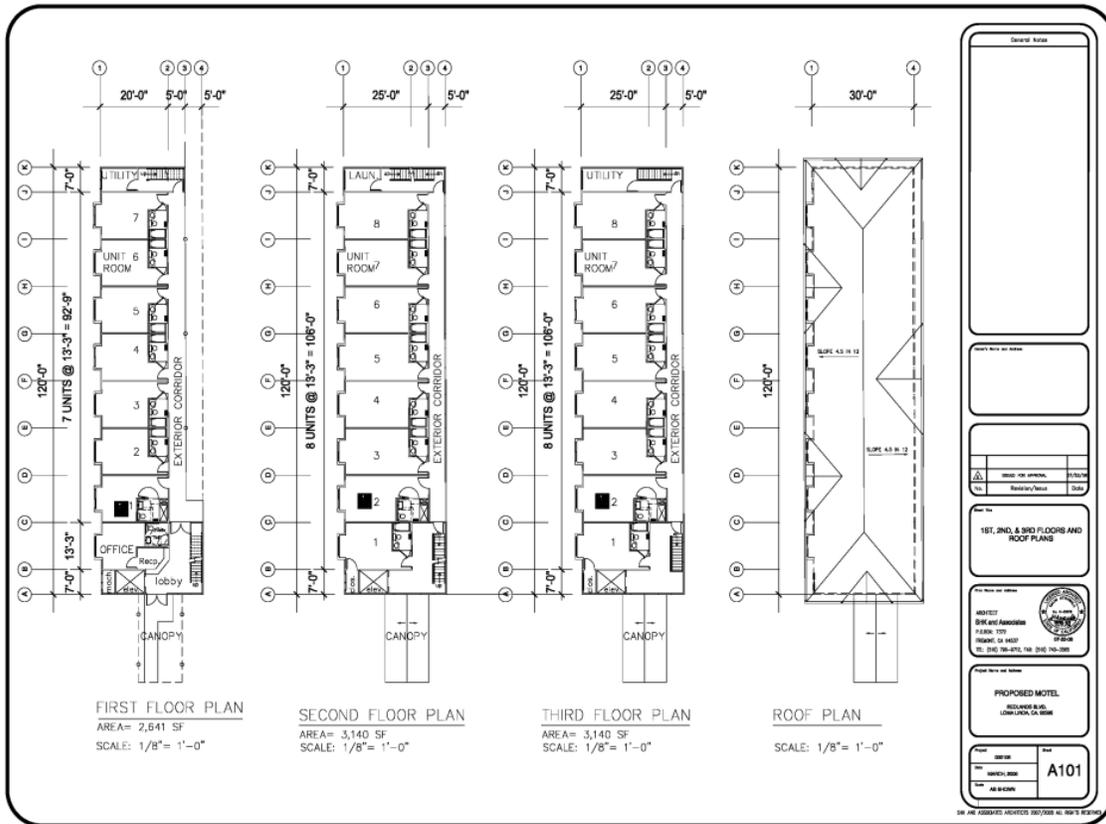
Blank Page

Blank Page

Figure 4



Blank Page



58 - ALL DIMENSIONS UNLESS OTHERWISE NOTED TO THE CONTRARY

Blank Page

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|---|--|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology /Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation/Traffic | <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |

DETERMINATION

On the basis of this initial evaluation:

- () I find that the proposed project COULD NOT have a significant effect on the environment. A NEGATIVE DECLARATION will be prepared.
- (✓) I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by, or agreed to, by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- () I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- () I find that the proposed project MAY have a "Potentially Significant Impact" or "Potentially Significant Unless Mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standard and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- () I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects 1) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and 2) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Prepared By: _____ Date: _____

Reviewed By: _____ Date: _____

EVALUATION OF ENVIRONMENTAL IMPACTS

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
1. AESTHETICS. <i>Would the project:</i>				
a) Have a substantial affect on a scenic vista?	()	()	()	(✓)
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a State Scenic Highway?	()	()	()	(✓)
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	()	()	()	(✓)
d) Create a new source of substantial light or glare, which would adversely affect day or nighttime views in the area?	()	(✓)	()	()

Comments:

- a/b) **No Impact.** According to the City’s General Plan, the project site is not within a scenic vista/scenic highway view corridor. Nearby streets including local portions of Redlands Boulevard and Anderson Street are not considered scenic routes. The proposed project includes the construction of the 23-unit motel and associated on-site improvements. The site has always been vacant so no historic buildings will be affected. Similarly, there are no rock outcroppings or trees that would be adversely affected by the proposed project.
- c) **No Impact.** The project site is flanked by existing residential and commercial structures and is bounded by a freeway corridor to the north and a major arterial street to the south. The proposed building would incorporate architectural elements that would blend with the adjacent commercial developments. The proposed project would not degrade the existing visual character of the site or its surroundings. No impact would result.
- d) **Less than Significant Impact when mitigations Incorporated.** The proposed 23-unit motel would not significantly increase the amount of light/glare currently generated by the adjacent commercial site. However, to ensure potential impacts to the existing residence located west of the site are reduced to a less than significant level, the following mitigation measure shall be implemented:
 1. **Prior to issuance of Certificate of Occupancy, the applicant shall submit a photometric plan and final lighting plan to City staff showing the exact locations of light poles and the proposed orientation and shielding of the fixtures to prevent glare onto existing homes to the east and potential residential development to the north.**

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
2. AGRICULTURAL RESOURCES. <i>Would the project:</i> a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	()	()	()	(✓)
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	()	()	()	(✓)
c) Involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland to non-agricultural use?	()	()	()	(✓)

Comments:

- a) **No Impact.** According to the General Plan Land Use Map, the site is designated Commercial. The project site and surrounding area has not been identified or designated as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency. No impacts to Farmland would result.
- b) **No Impact.** The proposed project is located on the north side of Redlands Boulevard, west of Anderson Street. Since, there is not an existing agricultural use or Williamson Act contract on the site, the proposed project and its location would not impact any agricultural land use or Williamson Act land conservation contract.
- c) **No Impact.** The proposed project does not involve other changes in the existing environment, which due to its location or nature, could result in conversion of Prime Farmland, to a non-agricultural use. Under the existing General Plan, there are no agricultural land use designations, although agriculture is an existing use in some areas of the City.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
3. AIR QUALITY. <i>Would the project:</i> a) Conflict with or obstruct implementation of the applicable air quality plan?	()	()	()	(✓)
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	()	()	(✓)	()

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable Federal or State ambient air quality standard (including releasing emissions, which exceed quantitative thresholds for ozone precursors?	()	()	(✓)	()
d) Expose sensitive receptors to substantial pollutant concentrations?	()	()	(✓)	()
e) Create objectionable odors affecting a substantial number of people?	()	()	()	(✓)

Comments:

- a) **Less than Significant Impact.** The project site is within the South Coast Air Basin and under the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The SCAQMD is responsible for updating the Air Quality Management Plan (AQMP). The AQMP was developed for the primary purpose of controlling emissions to maintain all federal and state ambient air standards for the district. The proposed project is consistent with the General Plan for which the EIR was prepared and impacts evaluated. The project would not conflict with South Coast Air Quality Management Plan. Therefore, no impact is anticipated.
- b/c) **Less than Significant Impacts.** The Air Quality and Climate Change Assessment was prepared by Hogle-Ireland, Inc. on January 31, 2012. Based on the CalEEMod output, criteria pollutant will not exceed SCAQMD thresholds of significance. Mitigation Measures are not required to reduce the project emissions below the thresholds identified in the table below for both construction and facility operation. The City of Loma Linda includes standard conditions on projects subject to CEQA to further reduce pollutant emissions, and have been added as mitigation measures for this project. As shown in the below, construction and operational emissions would not exceed SCAQMD thresholds for any criteria pollutants.

Criteria Pollutant

Activity	Criteria Pollutant Emissions (lbs/day)					
	ROG	NOx	CO	SO2	PM10	PM2.5
<i>Construction</i>						
Total	41.85	17.88	11.26	.02	2.03	1.57
Threshold	75	100	550	150	150	55
<i>Operational</i>						
Area	.23	0.00	0.00	0.00	0.00	0.00
Energy	.02	.15	.13	.01	.01	.01
Mobile	.57	1.31	5.19	0.01	.86	.06
Solid Waste	--	--	--	--	--	--
Water	--	--	--	--	--	--
Total	.82	1.46	5.32	0.01	0.87	0.07
Threshold	55	55	550	150	150	55

In a letter dated July 6, 2005, the SCAQMD recommended measures to reduce ROG emissions. Implementation of the recommendations would reduce ROG emissions to the greatest extent possible, and shall include the following:

- 2. The contractor shall use coating and solvents with a volatile organic compound (VOC) content lower than required under Rule 1113.**
- 3. The developer/contractor shall use building materials that do not require painting, where feasible.**
- 4. The developer/contractor shall use pre-painted construction materials where feasible.**

Although the proposed project does not exceed SCAQMD thresholds for construction emissions, the applicant is required to comply with all applicable SCAQMD rules and regulations as the South Coast Air Basin is in non-attainment status for ozone and suspended particulates (PM₁₀). The project shall comply with, Rules 402 nuisance, and 403, fugitive dust, which require the implementation of Best Available Control Measures (BACM) for each fugitive dust source, and the Air Quality Management Plan (AMCP), which identifies Best Available Control Technologies (BACT) for area sources and point sources, respectively. This would include, but not be limited to the following:

- 5. The project proponent shall ensure that any portion of the site to be graded shall be pre-watered prior to the onset of grading activities.**
 - (a) The project proponent shall ensure that watering of the site or other soil stabilization method shall be employed on an on-going basis after the initiation of any grading activity on the site. Portions of the site that are actively being graded shall be watered regularly to ensure that a crust is formed on the ground surface, and shall be watered at the end of each workday.**
 - (b) The project proponent shall ensure that all disturbed areas are treated to prevent erosion until the site is constructed upon.**
 - (c) The project proponent shall ensure that landscaped areas are installed as soon as possible to reduce the potential for wind erosion.**
 - (d) The project proponent shall ensure that all grading activities are suspended during first and second stage ozone episodes or when winds exceed 25 miles per hour.**

During construction, exhaust emissions from construction vehicles and equipment and fugitive dust generated by equipment traveling over exposed surfaces, would increase NO_x and PM₁₀ levels in the area. Although the proposed project does not exceed SCAQMD thresholds during construction, the applicant will be required to implement the following conditions as required by SCAQMD:

- 6. To reduce emissions, all equipment used in grading and construction must be tuned and maintained to the manufacturer's specification to maximize efficient burning of vehicle fuel.**
- 7. The project proponent shall ensure that existing power sources are utilized where feasible via temporary power poles to avoid on-site power generation during construction.**
- 8. The project proponent shall ensure that construction personnel are informed of ride sharing and transit opportunities.**

9. All buildings on the project site shall conform to energy use guidelines in Title 24 of the California Administrative Code.
 10. The operator shall maintain and effectively utilize and schedule on-site equipment in order to minimize exhaust emissions from truck idling.
 11. The operator shall comply with all existing and future CARB and SCAQMD regulations related to diesel-fueled trucks, which may include among others: (1) meeting more stringent emission standards; (2) retrofitting existing engines with particulate traps; (3) use of low sulfur fuel; and (4) use of alternative fuels or equipment.
- d) **Less than Significant Impacts.** Nearby sensitive receptors include the Loma Linda Academy (K-12) located approximately a ¼ mile southeast of the project site. An increase in air quality emissions produced as a result of construction activities would be short-term, below SCAQMD thresholds, and would cease once construction is complete. Dust suppression (i.e., water application) as required by the City’s Development Code, would reduce 50 to 75 percent of fugitive dust emissions during construction. Impacts are considered less than significant.
- e) **No Impact.** The proposed construction and operation of a 23-room motel is to be located on a property designated Commercial. The end use of the proposed project is not anticipated to generate emissions that could cause climatic changes or objectionable odors. No impact is anticipated.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
4. BIOLOGICAL RESOURCES. <i>Would the project:</i>				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	()	()	()	(✓)
b) Have a substantial adverse effect on riparian habitat or other sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	()	()	()	(✓)
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	()	()	()	(✓)
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	()	()	()	(✓)

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	()	(✓)	()	()
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community conservation Plan, or other approved local, regional, or State habitat conservation plan?	()	()	()	(✓)

Comments:

- a) **No Impact.** Critical habitat identifies specific areas that are essential to the conservation of a listed species and, with respect to areas within the geographic range occupied by the species. As shown on Figure 9.3 of the City’s General Plan, the project site does not occur within the proposed critical habitat for the California gnatcatcher or any other species of concern or listed species. According to Figure 9.2 of the General Plan, the site and surrounding area is developed.
- b) **No Impact.** According to Figure 9.2 of the General Plan, no riparian habitat occurs on or near the project site. Therefore, the project would not have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. The parcels surrounding the project site are currently developed and contain no such habitats.
- c) **No Impact.** The proposed project is a request to construct a new 23-room motel in an urbanized area. All areas within and adjacent to the project area were found to be highly disturbed. Additionally, the project site is not considered federally protected wetlands as defined by Section 404 of the Clean Water Act
- d) **No Impact.** The proposed project will not have any adverse effect, because the area is not identified as a protected path for the native residents or migratory fish or wildlife species.
- e) **Less than significant impact with mitigations incorporated.** The project would not conflict with any local policies or ordinances protecting biological resources, and there are no identified biological resources that are subject to such regulation. However, to ensure potential impacts from the removal of any trees is less than significant the following mitigation measure shall be implemented:
 - 12. Should the relocation or removal of any tree be required, the applicant shall submit an Arborist Report prior to site disturbance. Any removal or replacement of trees shall be in accordance with the City’s Tree Preservation Ordinance.**
- f) **No Impact.** The project would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan, because no such plan has been adopted for the project site or surrounding area.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
5. CULTURAL RESOURCES. <i>Would the project:</i> a) Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?	()	()	()	(✓)
b) Cause a substantial adverse change in the significance of an archeological resource pursuant to § 15064.5?	()	()	()	(✓)
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	()	(✓)	()	()
d) Disturb any human remains, including those interred outside of formal cemeteries?	()	(✓)	()	()

Comments:

a-b) **No Impact.** According to CEQA §15064.5 (b), “substantial adverse change in the significance of a historic resource means physical demolition, destruction, relocation, or alteration of the resource or its immediate surrounding such that the significance of a historical resource would be materially impaired”. In order to create such a substantial adverse change, the resource must possess historical significance.

In their 1988 Historical Study, the City identified four potential historic districts. The historic districts were established based on areas that contained concentrations of improvements with historic interest or value. The project site does not occur within any of the four identified historic districts. Additionally, no buildings have been constructed on the proposed project site and therefore no adverse affects to a designated historical resource would result.

c) **Less than Significant Impacts with Mitigation Incorporated.** According to Figure 4.5.1 of the City’s General Plan EIR, the project site occurs within an area that has an undetermined potential for paleontological resources. This determination was based on the lack of literature and records checks, and other field surveys relative to this site. Since the potential of unearthing vertebrate fossils is unknown, necessary measures should be taken to ensure impacts are minimized. The following mitigation measure shall be implemented by the construction contractor:

13. Should paleontological resources be uncovered during grading, a qualified vertebrate paleontologist shall be contracted to perform a field survey to determine and record any nonrenewable paleontologic resources found on-site. The paleontologist will determine the significance, and make recommendations for appropriate mitigation measures in compliance with the guidelines of the California Environmental Quality Act.

Implementation of the above mitigation measure would reduce impacts to potential paleontological resources to a less than significant level.

d) **Less than Significant Impacts with Mitigation Incorporated.** Construction activities, particularly grading, soil excavation and compaction, could adversely affect or eliminate existing and unknown potential archaeological resources. The following mitigation measures shall be implemented:

14. In the event that human remains are encountered during grading, all provisions of state law requiring notification of the County Coroner, contacting the Native American Heritage Commission, and consultation with the most likely descendant, shall be followed.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
6. GEOLOGY AND SOILS. <i>Would the project:</i> a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	()	()	(✓)	()
ii) Strong seismic ground shaking?	()	()	(✓)	()
iii) Seismic-related ground failure, including liquefaction?	()	()	(✓)	()
iv) Landslides?	()	()	()	(✓)
b) Result in substantial soil erosion or the loss of topsoil?	()	()	(✓)	()
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	()	()	(✓)	()
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	()	()	(✓)	()
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	()	()	()	(✓)

Comment :

- a) **Less than Significant Impact.** The City of Loma Linda is situated within the northern Peninsular Ranges Geomorphic Province of California. Locally, the City lies near the transition zone between the Transverse Ranges Geomorphic Province to the north and the Peninsular Ranges Geomorphic Province to the south. The Peninsular Ranges are a northwest-southeast oriented complex of blocks separated by similarly trending faults which extend 125 miles from the Transverse Ranges to south of the California/Mexican border and beyond another 775 miles to the tip of Baja California.
- i-iii) **Less than Significant Impact.** The Loma Linda General Plan indicates that the project site is not located within the Alquist-Priolo Earthquake Fault Zone and, therefore, does not require a geologic study to mitigate this naturally occurring

hazard throughout Southern California. The new addition would subject the users to geologic hazards such as earthquakes that occur from time to time in the Southern California area. The closest mapped fault is the Loma Linda Fault (inactive) that lies approximately 1/4 mile west of the project site. Southern California is a seismically active region; however, safety provisions identified in the California Building Code shall be required which will reduce potential ground shaking hazards to a level below significance. The project site is within an area which may be susceptible to the effects of liquefaction. However, with proper construction methods and development standards as defined in the Development Code and the latest adopted building regulations, the potential for structural damages will be mitigated.

- iv) **No Impact.** The occurrence of landslides is considered minimal because the project site is relatively flat and is not on or near a geologic formation that would cause landslides.
- b) **Less than Significant Impact.** The State of California is authorized to administer various aspects of the National Pollutant Discharge Elimination System (NPDES). Construction activities covered under the State’s General Construction permit include removal of vegetation, grading, excavation, or any other activity that causes the disturbance of one acre or more.

It is not anticipated that the development of this site will contribute to significant soil erosion or loss of topsoil. Some erosion will occur as a result of grading and the construction process, however, the implementation of Best Management Practices for erosion and sediment control will result in a less than significant impact in this area.

- c-d) **Less than Significant Impact.** As previously discussed, the project site occurs within a liquefaction hazard zone. However, the site and surrounding area is currently developed. The proposed project would not increase potential liquefaction hazards. Implementation of the proposed project would not result in soils becoming unstable.
- e) **No Impact.** The proposed development would connect to, and be served by, the existing local sewer system for wastewater disposal. The proposed development would be required to connect to the existing system. No septic tanks or alternative wastewater disposal is proposed.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
7. GREENHOUSE GAS EMISSIONS. <i>Would the project:</i> a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	()	()	(✓)	()
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	()	()	(✓)	()

- a) **Less than Significant Impact.** The Air Quality/Climate Change Assessment was prepared by Hogle-Ireland. Based on the CalEEMod output, the Assessment found that criteria pollutant and greenhouse gas emissions will not exceed the thresholds of significance identified in the table below:

Greenhouse Gas Emissions

Activity	GHG Emissions (mt/yr)
	CO2E
<i>Construction</i>	
Total	3.26
<i>Threshold</i>	N/A
<i>Operational</i>	
Area	0.00
Energy	83.05
Mobile	124.06
Solid Waste	5.73
Water	2.95
Total	219.05
<i>Threshold</i>	1,400

The project as conditioned, is not anticipated to conflict with an applicable plan, policy or regulation adopted for the purposes of reducing emissions of greenhouse gases in that the project complies with the latest threshold developed by SCAQMD using the 1,400 metric tons carbon dioxide equivalent (MTCO₂E) per year for commercial projects.

While mitigation measures are not required of the project, the City of Loma Linda includes standard conditions of approval on projects subject to CEQA to further reduce pollutant emissions. As such, the mitigation measures listed under Air Quality have been incorporated.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
8. HAZARDS AND WASTE MATERIALS. <i>Would the project:</i>				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	()	(✓)	()	()
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident considerations involving the release of hazardous materials into the environment?	()	(✓)	()	()
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within 1/4 mile of an existing or proposed school?	()	(✓)	()	()
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	()	()	()	(✓)

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within 2 miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	()	(✓)	()	()
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	()	()	()	(✓)
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	()	()	()	(✓)
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	()	()	()	(✓)

Comments:

a-c) **Less than Significant with Mitigation Incorporated.** A less than significant impact from hazardous materials transport or use will occur during construction activities at the project site. Hazardous materials, which may be present during construction, include limited storage of fuel and the storage of paints and solvents common to construction. Quantities of materials stored on-site during construction activities will be limited to amounts reasonable and necessary for construction activities and will be stored in a manner consistent with hazardous material storage requirements. Although potentially hazardous materials may be on site, the quantities and use of these materials is routine and will not pose a threat to surrounding areas or the public in general. However, a Phase I environmental Site Assessment should performed on the site prior to the issuance of building permits. Should any type of soil contamination be discovered prior to or during construction, the applicant shall apply the following mitigation measures to reduce the impact to less than significant:

15. The applicant shall perform a Phase I Environmental Site Assessment to determine if the project site includes any contamination prior to the issuance of building permits.
16. The applicant shall prepare a study for the presence of hazardous chemicals, mercury, and asbestos containing materials (ACM) as a result of the demolition of the existing on-site structures. If other hazardous chemicals, lead-based paints (LPB) or products, mercury or ACMs are identified, proper precautions should be taken during demolition activities. Additionally, the contaminants should be remediated in compliance with California environmental regulations and policies.
17. Should future project construction require soil excavation or filling in certain areas, soil sampling may be required. If soil is contaminated, it must be properly disposed. Land Disposal Restrictions (LDRs) may be applicable to such soils. Soil sampling shall also be conducted on any imported soil.

18. If it is determined that hazardous wastes are, or will be generated by the proposed operation of the facility, the wastes shall be managed in accordance with the California Hazardous Waste Control Law and the Hazardous Waste Control Regulations. If it is determined that hazardous wastes will be generated, the facility shall obtain a United States Environmental Protection Agency Identification Number. Certain hazardous waste treatment processes or hazardous materials, handling, storage or uses may require authorization from the local Certified Unified Program Agency (CUPA).

19. If clean up oversight is required of the project, the applicant shall be required to obtain an Environmental Oversight Agreement with the DTSC.

The project is not anticipated to emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste. No impacts would result to students at the Loma Linda Academy campus located approximately 1/4 mile north of the site.

- d) **No Impact.** This project is not on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5. Therefore, this project will not create a significant hazard to the public or the environment.
- e) **No Impact.** The site is not located within an airport land use plan and is not within two miles of a public airport. The nearest airports are the San Bernardino International Airport, located over three miles north of the project site, and the Redlands Municipal Airport, located approximately five miles northeast of the site. According to Figure 10.4 of the General Plan, the project site is located outside of the San Bernardino International Airport influence area. The proposed project would not create a safety hazard to people or aircraft.
- f) **No Impact.** There are no private airstrips within the vicinity of the project site.
- g) **No Impact.** The California Emergency Services Act requires the City to manage and coordinate the overall emergency and recovery activities within its jurisdictional boundaries. The City's Emergency Operations Plan includes policies and procedures to be administered by the City in the event of a disaster. During disasters, the City of Loma Linda is required to coordinate emergency operations with the County of San Bernardino. Policies within the City's General Plan and updates to the City's Emergency Plan, as required by State law, would ensure the proposed project would not interfere with adopted policies and procedures. No impact is anticipated.
- h) **No Impact.** The City of Loma Linda has defined areas susceptible to wildland fires by a boundary identified as the Urban Wildland Interface division line. According to Figure 10.3 of the General Plan, the greatest fire hazard can be expected to come from the adjacent hills and canyons in the southern portion of the City. The project site is located over 2 miles north of the nearest identified hazardous fire area. The project site is located within an urbanized area and is surrounded by development. The project would not expose people or structures to a significant risk of loss, injury or death involving wildland fires.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
9. HYDROLOGY AND WATER QUALITY. <i>Would the project:</i>	()	()	(✓)	()
a) Violate any water quality standards or waste discharge requirements?	()	()	(✓)	()
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	()	()	()	(✓)
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site?	()	()	()	(✓)
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site?	()	()	()	(✓)
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	()	()	()	(✓)
f) Otherwise substantially degrade water quality?	()	()	(✓)	()
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	()	()	()	(✓)
h) Place within a 100-year flood hazard area structures, which would impede or redirect flood flows?	()	()	()	(✓)
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	()	()	()	(✓)
j) Inundation by seiche, tsunami, or mudflow?	()	()	()	(✓)

Comments:

a,f) **Less than Significant Impact.** The State of California is authorized to administer various aspects of the National Pollutant Discharge Elimination System (NPDES).

Construction activities covered under the State's General Construction permit include removal of vegetation, grading, excavating, or any other activity that causes the disturbance of one acre or more. Since the proposed project would disturb approximately 0.4 acres, it is not subject to the NPDES permit requirements.

However, the development of the project site potentially may cause soil sedimentation and water pollution during grading and construction phases. Operations of the facility, including maintenance and irrigation can also lead to sedimentation and water contamination. An erosion/sediment control plan and a Water Quality Management Plan are required to address on-site drainage control during construction. The proposed project will increase the amount of impervious area thereby increasing the amount of potential runoff from the site. The increase in runoff will be less than significant and will not exceed the capacity of existing or planned stormwater drainage systems, or contribute a significant amount of pollutants to runoff. The proposed project will protect water quality by complying with City standards and a stormwater pollution prevention plan (SWPPP). The following mitigation measures shall be implemented to reduce the impact to less than significant:

20. **All site drainage shall be handled on-site and shall not be permitted to drain onto adjacent properties.**
 21. **Prior to issuance of grading permits, the applicant shall obtain coverage under the NPDES Statewide Industrial Stormwater Permit for General Construction Activities from the State Water Resources Control Board. Evidence that this has been obtained shall be submitted to the City of Loma Linda Public Works Department.**
 22. **An erosion/sediment control plan and a Water Quality Management Plan are required to address on-site drainage construction and operation.**
 23. **All necessary precautions and preventive measures shall be in place in order to prevent material from being washed away by surface waters or blown by wind. These controls shall include at a minimum: Regular wetting of surface or other similar wind control method, installation of straw or fiber mats to prevent rain related erosion. Detention basin(s) or other appropriately sized barrier to surface flow must be installed at the discharge point(s) of drainage from the site. Any water collected from these controls shall be appropriately disposed of at a disposal site. These measures shall be added as general notes on the site plan and a statement added that the operator is responsible for ensuring that these measures continue to be effective during the duration of the project construction.**
 24. **The project shall comply with the Low Impact Development (LID) Principles and LID Best Management Practices (BMPs) for Southern California.**
- b) **No Impact.** The City obtains all of its water from groundwater wells in the Bunker Hill Basin, an aquifer underlying the San Bernardino Valley. Groundwater in the Bunker Hill Basin is replenished from rainfall and snowmelt from the San Bernardino Mountains. The proposed project would not deplete groundwater supplies nor would it interfere with recharge since it is not within an area designated as a recharge basin or spreading ground. The proposed 23-unit motel would receive water supply directly from the City of Loma Linda whose source of supply is groundwater.
- c-e) **No Impact.** The proposed project would include paved areas and building coverage on-site; however, the project will not alter the course of any stream or river. All runoff would be conveyed to existing storm drain facilities, which have been designed to handle the flows. The project design includes landscaping of all non-hardscape areas to prevent erosion. The Building Official and City Engineer must approve a grading and drainage plan prior to the issuance of grading permits. Review and approval of the drainage plan

would ensure the project would not result in substantial erosion, siltation, or flooding on- or off-site.

- g) **No Impact.** The Federal Emergency Management Agency's Flood Insurance Rate Map (Letter of Map Revision Dated-November 19, 2007) identifies the project site as lying outside the 100-year floodplain. The proposed project will not impede or redirect flood flow. The proposed project will comply with the policies and requirements of the Loma Linda General Plan.
- h) **No Impact.** According to the revised FEMA Flood Insurance Rate Map, the project site is located within Zone X, which identifies areas with 0.2% annual chance of flooding, or areas with 1% annual chance of flooding of depths of less than one foot.
- i) **No Impact.** The San Bernardino County Flood Control District covers the entire County (including the incorporated cities), and provides planning, design, construction, and operation of flood control facilities. Storm drain systems have been constructed throughout the City of Loma Linda to accommodate both the increased runoff resulting from development and to protect developed areas within the City from potential localized flooding. The San Bernardino County Flood Control District has developed an extensive system of facilities, including dams, conservation basins, channels and storm drains to intercept and convey flood flows away from developed areas.
- j) **No Impact.** Due to the inland distance from the Pacific Ocean and any other significant body of water, tsunamis and seiching are not potential hazards; therefore impacts from seiche and tsunami are not anticipated.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
10. LAND USE AND PLANNING. <i>Would the project:</i>				
a) Physically divide an established community?	()	()	()	(✓)
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to, a general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	()	()	()	(✓)
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	()	()	()	(✓)

Comments:

- a-b) **No Impact.** The proposed 23-unit motel would be constructed on a property which is designated by the City of Loma Linda General Plan as Commercial. Land uses surrounding the project site include an existing fast food restaurant to the east, an existing non-conforming residential property to the west, and commercial uses to the south. Redlands Boulevard is the City's main commercial corridor. However, some properties within the commercial zone are non-conforming and were constructed to the development criteria of previous zoning standards, contributing to the eclecticism of the area. The project site is designated by the General Plan as Commercial and the proposed development would be consistent with uses permitted within the current designation and would not physically divide an established community.
- c) **No Impact.** The project would not conflict with any applicable habitat conservation plan or natural community conservation plan, because there is no habitat conservation plan

or natural community conservation plan within the area surrounding the project site and no habitat conservation lands are required to be purchased as mitigation for the proposed project.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
11. MINERAL RESOURCES. <i>Would the project:</i>				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the State?	()	()	()	(✓)
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	()	()	()	(✓)

Comments:

- a) **No Impact.** According to the California Department of Conservation, Division of Mines and Geology, the project site and surrounding area are designated Mineral Resource Zone 3 (MRZ-3). This designation is given for areas containing mineral deposits; the significance of which cannot be evaluated from available data due to urbanization. The proposed project would not result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the State due to urbanization and limited accessibility.
- b) **No Impact.** The project would not result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan, because there are no identified locally important mineral resources within the project area.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
12. NOISE. <i>Would the project result in:</i>				
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	()	(✓)	()	()
b) Exposure of persons to or generation of excessive ground borne vibration or ground borne noise levels?	()	()	()	(✓)
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	()	(✓)	()	()
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	()	()	(✓)	()

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within 2 miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	()	()	()	(✓)
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	()	()	()	(✓)

Comments:

a,c) **Less than Significant Impacts with Mitigation Incorporated.** The proposed 23-unit motel project will not generate noise levels in excess of standards established in the General Plan (July 25, 2006) which allows a maximum of 45dB and 65dB of interior and exterior noise levels respectively, or noise ordinance, or applicable standards of other agencies.

In 2007, a noise study was conducted by Gordon Bricken & Associates to determine compliance with state and local noise standards. Based on the analysis, the maximum unmitigated exterior noise level at the first floor was measured at 69 dB CNEL and 70 dB CNEL at the second and third floor levels respectively. This measurement will require a 24 dB and 25 dB structural reduction to achieve an interior noise level of 45 dB. No outdoor living area are included on the plans. The following mitigation measures shall also be implemented to help reduce the noise impacts to meet the City's interior (45dB) noise level.

25. The construction of the exterior wall shall incorporate the use of 7/8" stucco or siding, 2"x 4" studs, R-13 fiberglass insulation, and drywall.

26. Dual pane windows and entry doors with solid core wood and weather stripping construction shall be utilized.

27. Roof material shall consist of shingles or tile over sheathing construction, in addition to R-19 fiberglass insulation, drywall and venting.

The City's noise ordinance requires construction activities to be limited to the hours between 7:00 a.m. to 10:00 p.m. Monday through Friday, with no heavy construction occurring on weekends or national holidays. Additionally, all equipment is required to be properly equipped with standard noise muffling apparatus. Adhering to the City's noise ordinance and implementation of the above mitigation measure would ensure impacts from construction noise would be less than significant.

b) **No Impact.** Construction and operation of the 23-unit motel would not require the use of equipment which would generate excessive ground borne vibration or ground-borne noise levels. No impacts from ground-borne noise or vibration would result.

d) **Less than Significant impact.** The potential for disrupting persons in the vicinity of the project area is apparent due to the developed neighborhood surrounding the project site. However, during site construction, the project is required to comply with Section 9.20.050 (Prohibited Noises) of the Loma Linda Municipal Code, which requires that construction activities cease between the hours of 7:00 a.m. and 7:00 p.m. No additional mitigation is needed or proposed for short-term noise impacts.

- e) **No Impact.** The site is not located within an airport land use plan and is not within two miles of a public airport. The nearest airports are the San Bernardino International Airport, located approximately over three miles north of the project site, and the Redlands Municipal Airport, located approximately five miles northeast of the site. According to Figure 10.4 of the City’s General Plan, the project site is located outside of the San Bernardino International Airport influence area. The inhabitants of the proposed motels would not be exposed to any excessive noise from airport activities.
- f) **No Impact.** There are no private airstrips within the vicinity of the project site. No significant impacts from aircraft noise are anticipated.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
13. POPULATION AND HOUSING. <i>Would the project:</i> a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	()	()	()	(✓)
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	()	()	()	(✓)
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	()	()	()	(✓)

Comments:

- a) **No Impact.** The proposed project consists of the construction and operation of a 23-unit motel. Construction at the site would be short-term and would not create any new long-term construction jobs. Operation of the motel would require approximately 3 employees. According to Table 4.12 F of the City’s General Plan Update EIR, the City’s projected population, housing and employment levels upon build out would be less than the SCAG projections for the year 2025. The proposed project would be consistent with the General Plan, and therefore would not induce substantial population growth in an area, either directly or indirectly.
- b) **No Impact.** The proposed project would not displace any existing housing units, because no housing units would be demolished as a result of the proposed project.
- c) **No Impact.** The proposed project would not displace any people, or necessitate the construction of replacement housing elsewhere, because the project will not displace any existing housing or existing residents.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
14. PUBLIC SERVICES. <i>Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</i>				
a) Fire protection?	()	(✓)	()	()
b) Police protection?	()	(✓)	()	()
c) Schools?	()	(✓)	()	()
d) Parks?	()	()	(✓)	()
e) Other public facilities?	()	()	()	(✓)

Comments:

a) **Less than Significant Impact with Mitigations Incorporated.**

Fire Protection. Fire protection is provided by the Fire and Rescue Division of the Department of Public Safety, City of Loma Linda. Fire Station 251 serves the City and is located at 11325 Loma Linda Drive directly east of the project site across Loma Linda Drive. The Community Development Department and the Department of Public Safety enforce fire standards during review of building plans and inspections. The City maintains a joint response/automatic aid agreement with the fire departments in neighboring cities including Colton, Redlands, and San Bernardino. The Department also participates in the California Master Mutual Aid Agreement. The proposed motel would be required to comply with City fire suppression standards including building sprinklers and adequate fire access. Implementation of the following mitigation measure would ensure that the proposed project would not create a fire hazard or endanger the surrounding area.

28. The developer shall submit a Utility Improvement Plan showing the location of fire hydrants for review and approval by the Fire Department.

b) **Less than Significant Impact with Mitigation Incorporated.**

Police Protection. The San Bernardino County Sheriff's Department (SBSD) provides police protection for the City. The SBSB currently has 12 sworn officers assigned to the City. With an estimated population of 20,200 people, the ratio of officers to citizens is approximately 1:1,700. Due to the small scope of the project, a minimal increase in new population may be expected (a potential for three new residents to fulfill the three jobs that would be generated by the project). However, the proposed project would not significantly increase the population. In addition, the implementation of the following mitigation measures would ensure that no additional demand would be placed on officers to maintain the current level of service.

29. The developer shall provide sufficient exterior lighting to the site that illuminates otherwise dark corridors which may compromise public safety.

- 30. The developer shall be required to prevent loitering on site.
- 31. The developer shall be required to provide clear windows at the lobby area.
- 32. The developer shall register with the Crime Free Hotel/Motel program which closely works with San Bernardino County Sheriffs Department personnel to address crime prevention.

In addition to the aforementioned mitigations, the installation of a surveillance camera is also suggested.

c) Less than Significant Impact with Mitigations Incorporated.

Schools. The proposed project will not require additional school facilities for the local school district. School services within the City of Loma Linda are provided by the Redlands Unified School District and the Colton Joint Unified School District. The City mitigates impacts on school services through the collection of development fees. Under Section 65995 of the California Government Code, school districts may charge development fees to help finance local school services. The code prohibits State or local agencies from imposing school impact fees, dedications, or other requirements in excess of the maximum allowable fee, which is currently \$2.63 per square foot of new residential development and \$0.42 per square foot for commercial or other development for Redlands Unified School District. The following mitigation measure would ensure impacts are reduced to a less than significant level:

- 33. Prior to issuance of building permits, the developer shall pay school impact fees as required by the Redlands Unified School District.

d) Less than Significant Impact.

Parks. The proposed project will not adversely impact other publicly maintained facilities due to the limited size and scope of the project.

e) No Impact.

Other Public Facilities. Construction and operation of the 23-unit motel would not significantly increase traffic on adjacent streets (see Section 15 Transportation/Traffic). The proposed project would not require additional road maintenance by the City.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
15. RECREATION. <i>Would the project:</i> a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	()	()	(✓)	()
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	()	()	(✓)	()

Comments:

- a-b) **Less than Significant Impact.** The City has adopted a population to parkland acreage ratio of five acres per 1,000 population. The proposed project would generate approximately 3 jobs. Assuming that 3 jobs would be filled by new residents, an

additional 0.015 acres of parkland would be required for the City to maintain its policy of five acres of parkland per 1,000 residents. Therefore, the proposed project would contribute to the City's current insufficient parkland ratio. The new site would be required to develop landscaping and open space areas to off-set the impacts to parkland sites.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
16. TRANSPORTATION/TRAFFIC. <i>Would the project:</i> a) Cause an increase in traffic, which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	()	()	(✓)	()
b) Exceed, either individually or cumulatively, a level of service standard established by City's General Plan (as amended by Measure V) or the county congestion management agency for designated roads or highways?	()	()	()	(✓)
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	()	()	()	(✓)
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	()	()	()	(✓)
e) Result in inadequate emergency access?	()	()	()	(✓)
f) Result in inadequate parking capacity?	()	()	()	(✓)
g) Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?	()	()	()	(✓)

Comments:

a, b) **Less than Significant Impact.** In December 2011, Clyde Sweet and Associates prepared a Focused Traffic Impact Analysis (FTIA) for the proposed motel project. The purpose of the FTIA was to evaluate the traffic and circulation impacts of the proposed project. A total of four major intersections were evaluated in the FTIA. Trip generation estimates for the proposed project were developed using trip rates provided in the Institute of Transportation Engineers (ITE) Trip Generation, 7th Edition and distributed based on measured traffic data and street layout. Project Average Daily Trips (ADT) anticipated for the 23-unit motel is 126 over a 24-hour period. Project traffic trips were estimated to be 10 trips during the a.m. peak hour and 11 trips occurring during the p.m. peak hour. Based on the ITE formula, the

Level of Service (LOS) was also evaluated in the FTIA using Highway Capacity Manual (HCM 2000) methodology. As concluded in the report, all study intersections would continue to operate with satisfactory LOS (LOS C or better) with the addition of project traffic. Given the minimum amount of traffic to be generated by the proposed project, no

significant impacts from a substantial increase in either the number of vehicle trips or the volume to capacity ratio on roads would result; therefore impacts are determined to be less than significant. Based on the preceding, the proposed project complies with the traffic level of service requirements established in Measure V and therefore is consistent with the General Plan.

- c) **No Impact.** The site is not located within an airport land use plan and is not within two miles of a public airport. The nearest airports are the San Bernardino International Airport, located over three (3) miles north of the project site, and the Redlands Municipal Airport, located approximately five (5) miles northeast of the site. According to Figure 10.4 of the City's General Plan, the project site is located outside of the San Bernardino International Airport influence area. The proposed motel project would not change air traffic patterns or create a safety hazard to people or aircraft. No impacts would result.
- d) **No Impact.** The project will not result in a substantial increase in hazards due to a design feature. The proposed project will be compatible with the surrounding land uses and will utilize a driveway access from an existing City street (Redlands Boulevard). Improvements such as a City approved drive aisle, curb cut, and sidewalk dedication will be made to Redlands Boulevard to provide better ingress and egress to the project site.
- e) **No Impact.** The project will not result in inadequate emergency access. As previously stated, the project is subject to the requirements of the City's Public Works and Fire Departments. The project will be required to provide infrastructure that meets the performance requirements of all emergency vehicles. Access to the project site is proposed from Redlands Boulevard.
- f) **No Impact.** The project provides 27 total parking spaces for the proposed 23-unit motel. According to parking requirements for motels in the East Valley Corridor Specific Plan 1.1 spaces are required for each guest room. Based on the calculation 25 spaces are required. Therefore, no impact is anticipated.
- g) **No Impact.** The development will not cause a hazard or barrier to pedestrians or cyclists because adequate points of ingress/egress have been provided and there is adequate parking for residents and visitors onsite.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
17. UTILITIES AND SERVICE SYSTEMS. <i>Would the project:</i> a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	()	()	()	(✓)
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	()	()	()	(✓)
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	()	()	()	(✓)
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	()	()	()	(✓)

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
e) Result in a determination by the wastewater treatment provider, which serves or may serve the project, that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	()	()	()	(✓)
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	()	()	()	(✓)
g) Comply with Federal, State, and local statutes and regulations related to solid waste?	()	(✓)	()	()

Comments:

- a) **No Impact.** The City of Loma Linda's wastewater is treated by the City of San Bernardino through a Joint Powers Agreement (JPA). The City of San Bernardino operates both a secondary and a tertiary plant that discharge effluent to the Santa Ana River. Based on final calibrated field flow measurements for institutional land uses as listed in the City's Sanitary Sewer Master Plan, the project is projected to generate 3,291 gallons per day (gpd) (83.4 gpd per 1,000 square feet). Over six million gallons per day (MGD) of capacity exists at both San Bernardino plants. The proposed project will generate wastewater that can be discharged to a municipal system. The project is required to meet the requisites of the Santa Ana Regional Water Quality Control Board regarding wastewater.
- b) **No impact.** As previously stated, the City of San Bernardino under a JPA provides wastewater treatment services to the City of Loma Linda. Based on projected wastewater of 3,291 gallons per day, the proposed project would not require the expansion of existing facilities. The project will be served by existing City of Loma Linda sewer lines located along Redlands Boulevard. The proposed project will require connection to the existing system. According to the Public Works Department, sufficient capacity exists in the Redlands Boulevard sewer line.
- c) **No Impact.** The project site and surrounding area is currently served by existing storm drains. Drainage plans would be reviewed by the City Engineer to ensure the system will have sufficient carrying capacity to meet the proposed project demands. No impact is anticipated.
- d) **No Impact.** The production and distribution of water within the City of Loma Linda is provided by the City's Department of Public Works, Water Division. The City's groundwater is supplied from six wells. The total production capacity of these wells totals 7,900 gallons per minute. In addition to the groundwater wells, the City has two emergency connections with the City of San Bernardino and one with the City of Redlands. The City has the ability to finance and construct required facilities necessary to obtain the water supply to meet planned growth through the collection of development fees and the use of other funding methods.
- f) **No Impact.** The City contracts with Waste Management, Inc. of the Inland Empire to provide solid waste collection services. Solid waste not diverted to recycling or composting facilities is transported to the San Timoteo Sanitary Landfill within the City of Redlands. The San Timoteo Sanitary Landfill has a total permitted capacity of 20,400,000 cubic yards and has an estimated closure date of May 2016. The proposed project would not be served by a landfill with insufficient permitted capacity.

g) **Less than Significant Impact with Mitigations Incorporated.** As required by Assembly Bill 939 (AB939) of the California Integrated Waste Management Act, all cities and counties within the state must divert 50 percent of their wastes from landfills by the year 2000. According to tonnage reports, the City has not yet met the 50 percent diversion mandate. Construction & Demolition debris represents a large portion of materials being disposed of at landfills. To achieve the State-mandated diversion goal, the City has implemented a variety of programs that seek to reduce the volume of solid waste generated, encourage reuse, and support recycling efforts. City programs include the distribution of educational materials to local schools and organizations. The City also requires all applicable projects to comply with Resolution No. 2129 Construction and Demolition Recycling/Reuse Policy as adopted by the City Council. To ensure the proposed project contributes towards the diversion mandate, the following mitigation measure shall be implemented:

34. The project proponent shall comply with City adopted policies regarding the reduction of construction and demolition (C&D) materials.

Issues and Supporting Information Sources:	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>18. MANDATORY FINDINGS OF SIGNIFICANCE</p> <p>a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?</p>	()	()	(✓)	()
<p>b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?</p>	()	(✓)	()	()
<p>c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?</p>	()	()	(✓)	()

Comments:

a) **No Impact.** Critical habitat identifies specific areas that are essential to the conservation of a listed species and, with respect to areas within the geographic range occupied by the species. As shown on Figure 9.4 of the City's General Plan, the project site does not occur within the proposed critical habitat for the California gnatcatcher or any other species of concern or listed species. According to Figure 9.3 of the General Plan, the site and surrounding area is developed and includes urban landscaping.

According to Figure 9.3 of the General Plan, no riparian habitat occurs on or near the project site. Therefore, the project would not have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and

Wildlife Service. The properties located adjacent to the project site are currently developed and contains no such habitats.

In their 1988 Historical Study, the City identified four potential historic districts. The historic districts were established based on areas that contained concentrations of improvements with historic interest or value. The project site does not occur within any of the four identified historic districts. Additionally, no structures were constructed on the project site and therefore no adverse affects to a designated historical resource would result. No impacts are anticipated.

- b) **Less than Significant Impact with Mitigations Incorporated.** Several of the potential impacts identified in this Initial Study potentially have cumulatively considerable effects, which could degrade the quality of the environment if they are not avoided or sufficiently mitigated. Mitigation measures have been proposed and implementation of these mitigation measures will provide safeguards to prevent potentially significant cumulative impacts.
- c) **Less than Significant.** The proposed project and construction of the project will not impact long term environmental goals. The project proposes to construct a new 9,000 square-foot, 23-unit motel. It conforms to the surrounding uses and is consistent with the designate Commercial use. Similar to any development, the project is expected to expose residents to noise levels, traffic, light and glare that are above normal during the demolition and construction phases. However, the cumulative effects of these impacts will be less than significant.

Proposed development at the site would not cause substantial long-term adverse effects on human beings, either directly or indirectly. Construction activities would temporarily increase ambient noise levels for the surrounding area. The City's noise ordinance requires construction activities to be limited to the hours between 7:00 a.m. to 10:00 p.m. Monday through Friday, with no heavy construction occurring on weekends or national holidays. Additionally, all equipment is required to be properly equipped with standard noise muffling apparatus. Sensitive receptors including on-site employees and patients would be temporarily exposed to noise levels above City thresholds. Adhering to the City's noise ordinance and implementation of mitigation measures within this Initial Study would ensure impacts from construction and operational noise would be less than significant.

EARLIER ANALYSES

Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration per Section 15063(c)(3)(D). The effects identified above for this project were within the scope of and adequately analyzed in the following earlier document(s) pursuant to applicable legal standards, and such effects were addressed by mitigation measures based on the earlier analysis. The following earlier analyses were utilized in completing this Initial Study and are available for review in the City of Loma Linda, Planning Department:

- City of Loma Linda General Plan, June 2006
- City of Loma Linda Final General Plan Update Environmental Impact Report, LSA Associates.
- Air Quality and Climate Change Assessment, Hogle Ireland, January, 2012
- Updated Focused Traffic Impact Study, The Proposed 23-Unit Motel "Loma Linda Inn and Suites" on Redlands Boulevard in Loma Linda, CA, Clyde Sweet and Associates, December 2011.
- Focused Traffic Impact Study, The Proposed 23-Unit Motel "Loma Linda Inn and Suites" on Redlands Boulevard in Loma Linda, CA, Clyde Sweet and Associates, July 2007
- Acoustical Analysis, Gordon Bricken & Associates, July 26, 2007.

Blank Page

**CONDITIONS OF APPROVAL (REVISED)
PRECISE PLAN OF DESIGN (PPD) NO. 11-105**

COMMUNITY DEVELOPMENT DEPARTMENT

General

1. Within two years of this approval, the Precise Plan of Design shall be exercised by substantial construction or the permit/approval shall become null and void. In addition, if after commencement of construction, work is discontinued for a period of one year, the permit/approval shall become null and void.

PROJECT:

EXPIRATION DATE:

PRECISE PLAN OF DESIGN (PPD) NO. 11-105

May 16, 2014

2. The review authority may, upon application being filed 30 days prior to the expiration date and for good cause, grant a one-time extension not to exceed 12 months. The review authority shall ensure that the project complies with all current Development Code provisions.
3. In the event that this approval is legally challenged, the City will promptly notify the applicant of any claim or action and will cooperate fully in the defense of the matter. Once notified, the applicant agrees to defend, indemnify, and hold harmless the City, Redevelopment Agency (RDA), their affiliates officers, agents and employees from any claim, action or proceeding against the City of Loma Linda. The applicant further agrees to reimburse the City and RDA of any costs and attorneys fees, which the City or RDA may be required by a court to pay as a result of such action, but such participation shall not relieve applicant of his or her obligation under this condition.
4. Construction shall be in substantial conformance with the plan(s) approved by the Planning Commission. Minor modification to the plan(s) shall be subject to approval by the Director through a minor administrative variation process. Any modification that exceeds 10% of the following allowable measurable design/site considerations shall require the refiling of the original application and a subsequent hearing by the appropriate hearing review authority if applicable:
 - a. On-site circulation and parking, loading and landscaping;
 - b. Placement and/or height of walls, fences and structures;
 - c. Reconfiguration of architectural features, including colors, and/or modification of finished materials that do not alter or compromise the previously approved theme; and,
 - d. A reduction in density or intensity of a development project.
5. No vacant, relocated, altered, repaired or hereafter erected structure shall be occupied or no change of use of land or structure(s) shall be inaugurated, or no new business commenced as authorized by this permit until a Certificate of Occupancy has been issued by the Building Division. A Temporary Certificate of

Occupancy may be issued by the Building Division subject to the conditions imposed on the use, provided that a deposit is filed with the Community Development Department prior to the issuance of the Certificate, if necessary. The deposit or security shall guarantee the faithful performance and completion of all terms, conditions and performance standards imposed on the intended use by this permit.

6. This permit or approval is subject to all the applicable provisions of the Loma Linda Municipal Code, Title 17 in effect at the time of approval, and includes development standards and requirements relating to: dust and dirt control during construction and grading activities; emission control of fumes, vapors, gases and other forms of air pollution; glare control; exterior lighting design and control; noise control; odor control; screening; signs, off-street parking and off-street loading; and, vibration control. Screening and sign regulations compliance are important considerations to the developer because they will delay the issuance of a Certificate of Occupancy until compliance is met. Any exterior structural equipment, or utility transformers, boxes, ducts or meter cabinets shall be architecturally screened by wall or structural element, blending with the building design and include landscaping when on the ground.
7. Signs are not approved as a part of this permit. Prior to establishing any new signs, the applicant shall submit an application, and receive approval, for a sign permit from the Planning Division (pursuant to LLMC, Chapter 17.18) and building permit for construction of the signs from the Building Division, as applicable.
8. The applicant shall comply with all of the Public Works Department requirements for recycling prior to issuance of a Certificate of Occupancy.
9. Prior to issuance of Certificate of Occupancy, the applicant shall submit a photometric plan and final lighting plan to City staff showing the exact locations of light poles and the proposed orientation and shielding of the fixtures to prevent glare onto the existing home to the east.
10. During construction of the site, the project shall comply with Section 9.20 (Prohibited Noises) which limit construction activities to the hours between 7:00 a.m. to 10:00 p.m. Monday through Friday, with no heavy construction occurring on weekends or national holidays. Additionally, all equipment is required to be properly equipped with standard noise muffling apparatus. Adhering to the City's noise ordinance and implementation of the above mitigation measure would ensure impacts from construction noise would be less than significant.
11. The following mitigation measures shall also be implemented to help reduce the noise impacts to meet the City's interior (45dB) noise level.
 - a. The construction of the exterior wall shall incorporate the use of 7/8" stucco or siding, 2"x 4" studs, R-13 fiberglass insulation, and drywall.
 - b. Dual pane windows and entry doors with solid core wood and weather stripping construction shall be utilized.
 - c. Roof material shall consist of shingles or tile over sheathing construction, in addition to R-19 fiberglass insulation, drywall, and venting.

12. The applicant shall implement SCAQMD Rule 403 and standard construction practices during all operations capable of generating fugitive dust, which will include but not be limited to the use of best available control measures and reasonably available control measures such as:
 - a. Water active grading areas and staging areas at least twice daily as needed;
 - b. The project proponent shall ensure that all disturbed areas are treated to prevent erosion until the site is constructed upon.
 - c. The project proponent shall ensure that landscaped areas are installed as soon as possible to reduce the potential for wind erosion.
 - d. Suspend grading activities when wind gusts exceed 25 mph;
 - e. Sweep public paved roads if visible soil material is carried off-site;
 - f. Enforce on-site speed limits on unpaved surface to 15 mph; and
 - g. Discontinue construction activities during Stage 1 smog episodes.
13. The applicant shall implement the following construction practices during all construction activities to reduce VOC emission as stipulated in the project Initial Study and identified as mitigation measures:
 - a. The contractor shall utilize (as much as possible) pre-coated building materials and coating transfer or spray equipment with high transfer efficiency, such as high volume, low pressure (HVLP) spray method, or manual coating applications such as paint brush, hand roller, trowel, dauber, rag, or sponge.
 - b. The contractor shall utilize water-based or low VOC coating of 100 g/l of VOC (allowing approximately 31,500 square feet painted per day) to 250 g/l of VOC (allowing approximately 12,950 square feet painted per day). The following measures shall also be implemented:
 - Use Super-Compliant VOC paints whenever possible.
 - If feasible, avoid painting during peak smog season: July, August, and September.
 - Recycle leftover paint. Take any left over paint to a household hazardous waste center; do not mix leftover water-based and oil-based paints.
 - Keep lids closed on all paint containers when not in use to prevent VOC emissions and excessive odors.
 - For water-based paints, clean up with water only. Whenever possible, do not rinse the clean-up water down the drain or pour it directly into the ground or the storm drain. Set aside the can of clean-up water and take it to a hazardous waste center (www.cleanup.org).
 - Recycle the empty paint can.
 - Look for non-solvent containing stripping products.
 - Use Compliant Low-VOC cleaning solvents to clean paint application equipment.

- Keep all paint and solvent laden rags in sealed containers to prevent VOC emissions.
 - The developer/contractor shall use building materials that do not require painting, where feasible.
 - The developer/contractor shall use pre-painted construction materials where feasible.
14. The applicant shall work with the City's franchised solid waste hauler to follow a debris management plan to divert the material from landfills by the use of separate recycling bins (e.g., wood, concrete, steel, aggregate, glass) during demolition and construction to minimize waste and promote recycle and reuse of the materials.
 15. To reduce emissions, all equipment used in grading and construction must be tuned and maintained to the manufacturer's specification to maximize efficient burning of vehicle fuel.
 16. The project proponent shall ensure that existing power sources are utilized where feasible via temporary power poles to avoid on-site power generation during construction.
 17. The project proponent shall ensure that construction personnel are informed of ride sharing and transit opportunities.
 18. The operator shall maintain and effectively utilize and schedule on-site equipment in order to minimize exhaust emissions from truck idling.
 19. The operator shall comply with all existing and future CARB and SCAQMD regulations related to diesel-fueled trucks, which may include among others: (1) meeting more stringent emission standards; (2) retrofitting existing engines with particulate traps; (3) use of low sulfur fuel; and (4) use of alternative fuels or equipment.
 20. The proposed project shall contribute on a fair share basis, through an adopted traffic impact fee schedule, in the implementation of the recommended intersection lane improvements or in dollar equivalent in lieu mitigation contributions, or in the implementation of additional capacity on parallel routes to offset potential impacts to study area intersections as listed the Traffic Impact Analysis.
 21. All Development Impact fees shall be paid to the City of Loma Linda prior to the issuance of any building and/or construction permits.
 22. Prior to issuance of any Building and/or Construction Permits, the applicant shall submit to the Community Development Department proof of payment or waiver from both the City of San Bernardino for sewer capacity fees and Redlands Unified School District for school impact fees.
 23. The applicant shall pay all required development impact fees to cover 100 percent of the pro rata share of the estimated cost of public infrastructure, facilities, and services.
 24. The developer shall provide infrastructure for the Loma Linda Connected Community Program, which includes providing a technologically enabled

development that includes coaxial, cable and fiber optic lines to all outlets in each unit of the development. Plans for the location of the infrastructure shall be provided with the precise plan of design, which includes providing a technologically enabled development that includes coaxial, cable, and fiber optic lines to all outlets in each unit of the development. Plans for the location of the infrastructure shall be provided with the precise grading plans and reviewed and approved by the City of Loma Linda prior to issuing grading permits.

25. The project shall comply with the City Art in Public Places Ordinance (LLMC Chapter 17.26), which establishes grounds for compliance for new enterprises to facilitate public art. The establishment of artistic assets will be financed and/or constructed by the development community as part of the development requirements.
26. Should paleontological resources be uncovered during grading, a qualified vertebrate paleontologist shall be contracted to perform a field survey to determine and record any nonrenewable paleontological resources found on-site. The paleontologist will determine the significance, and make recommendations for appropriate mitigation measures in compliance with the guidelines of the California Environmental Quality Act.
27. In the event that human remains are encountered during grading, all provisions of state law requiring notification of the County Coroner, contacting the Native American Heritage Commission, and consultation with the most likely descendant, shall be followed.
28. The project shall comply with all non-exempt provisions of Measure V and shall pay the full amount of any recalculated development impact fees, including traffic impact fees, prior to occupancy.

Landscaping

29. The applicant shall submit three sets of the final landscape plan prepared by a state licensed Landscape Architect, subject to the approval of the Community Development Department, and Public Works Department for landscaping in the public right-of-way. Landscape plans for the Landscape Maintenance District shall be on separate plans.
30. Final landscape and irrigation plans shall be in substantial conformance with the approved conceptual landscape plan and these conditions of approval. Any and all fencing shall be illustrated on the final landscape plan.
31. Landscape plans shall depict the utility laterals, concrete improvements, and tree locations. Any modifications to the landscape plans shall be reviewed and approved by the Public Works and Community Development Departments prior to issuance of permits.
32. The applicant, property owner, and/or business operator shall maintain the property and landscaping in a clean and orderly manner and all dead and dying plants shall be replaced with similar or equivalent type and size of vegetation.

33. Should the relocation or removal of any tree be required, the applicant shall submit an Arborist Report prior to site disturbance. Any removal or replacement of trees shall be in accordance with the City's Tree Preservation Ordinance.
34. The applicant shall perform a Phase I Environmental Site Assessment to determine if the project site includes any contamination prior to the issuance of building permits.
35. The applicant shall prepare a study for the presence of hazardous chemicals, mercury, and asbestos containing materials (ACM) as a result of the demolition of the existing on-site structures. If other hazardous chemicals, lead-based paints (LPB) or products, mercury or ACMs are identified, proper precautions should be taken during demolition activities. Additionally, the contaminants should be remediated in compliance with California environmental regulations and policies.
36. Should future project construction require soil excavation or filling in certain areas, soil sampling may be required. If soil is contaminated, it must be properly disposed. Land Disposal Restrictions (LDRs) may be applicable to such soils. Soil sampling shall also be conducted on any imported soil.
37. If it is determined that hazardous wastes are, or will be generated by the proposed operation of the facility, the wastes shall be managed in accordance with the California Hazardous Waste Control Law and the Hazardous Waste Control Regulations. If it is determined that hazardous wastes will be generated, the facility shall obtain a United States Environmental Protection Agency Identification Number. Certain hazardous waste treatment processes or hazardous materials, handling, storage or uses may require authorization from the local Certified Unified Program Agency (CUPA).
38. If clean up oversight is required of the project, the applicant shall be required to obtain an Environmental Oversight Agreement with the DTSC.

FIRE DEPARTMENT

39. The applicant shall submit a complete set of plans to the Loma Linda Fire Department for review and approval prior to the issuance of building permits.
40. All construction shall meet the requirements of the editions of the California Building Code (CBC) and the California Fire Code (CFC)/International Fire Code (IFC) as adopted and amended by the City of Loma Linda and legally in effect at the time of issuance of building permit.
41. Pursuant to CFC Section 903, as amended in Loma Linda Municipal Code (LLMC) Sections 15.28.230-450, the building(s) shall be equipped with automatic fire sprinkler system(s). Pursuant to CFC Section 901.2, plans and specifications for the fire sprinkler system(s) shall be submitted to Fire Prevention for review and approval prior to installation. Fire flow test data for fire sprinkler calculations must be current within the last 6 months. Request flow test data from Loma Linda Fire Prevention.
42. On-site civil engineering improvement plans shall be submitted to Fire Prevention for review and approval prior to construction. Plans shall show the proposed

locations for water mains and fire hydrants; driveways, drive aisles and access roadways for fire apparatus.

43. The site address shall be as assigned by the Fire Marshal in a separate document, following approval of the project, and upon submittal of a working copy of the final approved site plan.
44. The developer shall submit a Utility Improvement Plan showing the location of fire hydrants for review and approval by the Fire Department.
45. Pursuant to CFC Section 907, all new buildings and structures shall be equipped with an approved Fire Alarm system installed in accordance with provisions of this code and NFPA 72. Plans for the system shall be submitted to the Fire Department for review and approval prior to the issuance of building permits.

PUBLIC WORKS DEPARTMENT

46. The developer shall submit an engineered grading plan for the proposed project.
47. All utilities shall be underground. The City of Loma Linda shall be the sewer purveyor.
48. All public improvement plans shall be submitted to the Public Works Department for review and approval.
49. Any damage to existing improvements as a result of this project shall be repaired by the applicant to the satisfaction of the City Engineer.
50. Prior to issuance of grading permits, the applicant shall submit to the City Engineer a Notice of Intent (NOI) to comply with obtaining coverage under the National Pollutant Discharge Elimination System (NPDES) General Construction Storm Water Permit from the State Water Resources Control Board. Evidence that this has been obtained (i.e., a copy of the Waste Dischargers Identification Number) shall be submitted to the City Engineer for coverage under the NPDES General Construction Permit.
51. All site drainage shall be handled on-site and shall not be permitted to drain onto adjacent properties.
52. An erosion/sediment control plan and a Water Quality Management Plan are required to address on-site drainage construction and operation.
53. All necessary precautions and preventive measures shall be in place in order to prevent material from being washed away by surface waters or blown by wind. These controls shall include at a minimum: regular wetting of surface or other similar wind control method, installation of straw or fiber mats to prevent rain related erosion. Detention basin(s) or other appropriately sized barrier to surface flow must be installed at the discharge point(s) of drainage from the site. Any water collected from these controls shall be appropriately disposed of at a disposal site. These measures shall be added as general notes on the site plan and a statement added that the operator is responsible for ensuring that these measures continue to be effective during the duration of the project construction.

54. Per the City of Loma Linda recycling policy, the project proponent shall incorporate interior and exterior storage areas for recyclables.
55. The project proponent shall comply with City adopted policies regarding the reduction of construction and demolition (C&D) materials.
56. The project shall comply with the Low Impact Development (LID) Principles and LID Best Management Practices (BMPs) for Southern California.

SHERIFFS DEPARTMENT

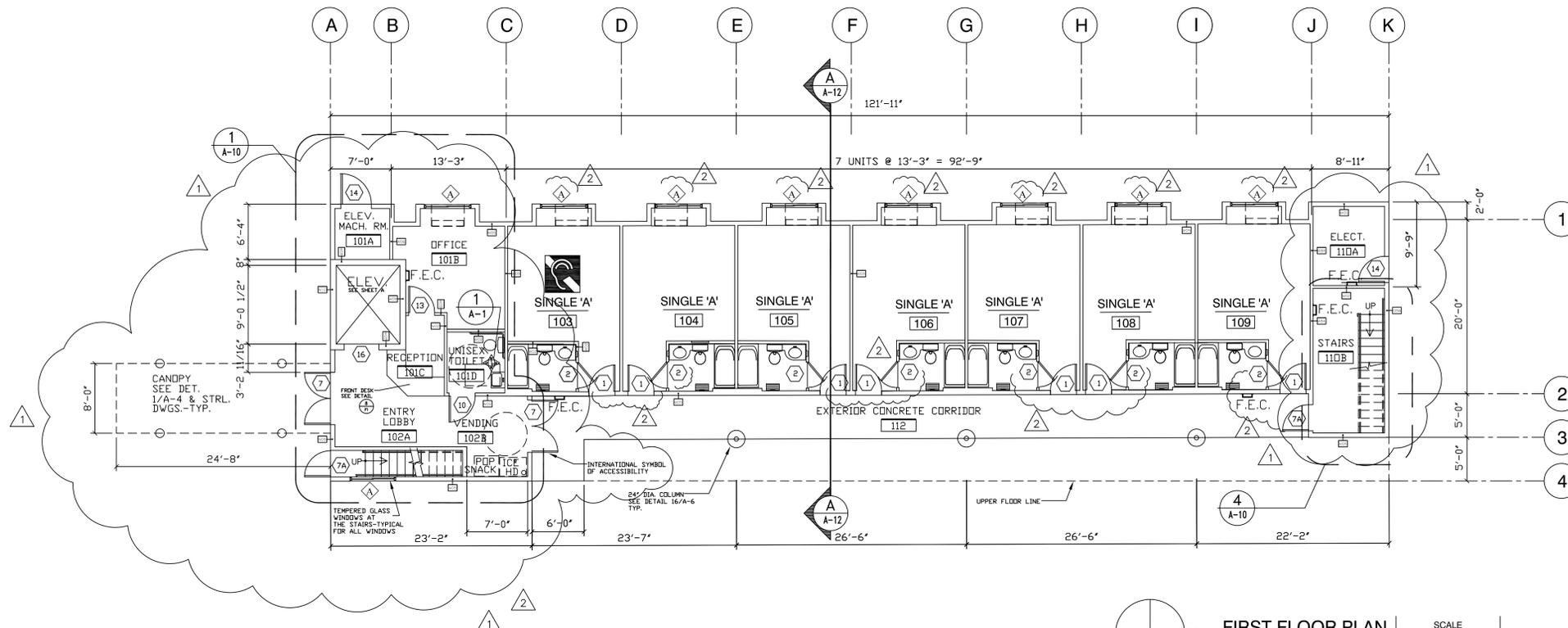
57. The developer shall provide sufficient exterior lighting to the site that illuminates otherwise dark corridors which may compromise public safety.
58. The developer shall register with the Crime Free Hotel/Motel Program which closely works with San Bernardino County Sheriffs Department personnel to address crime prevention.
59. The developer shall be required to prevent loitering on site.
60. The developer shall be required to provide clear windows at the lobby area.

Applicant signature

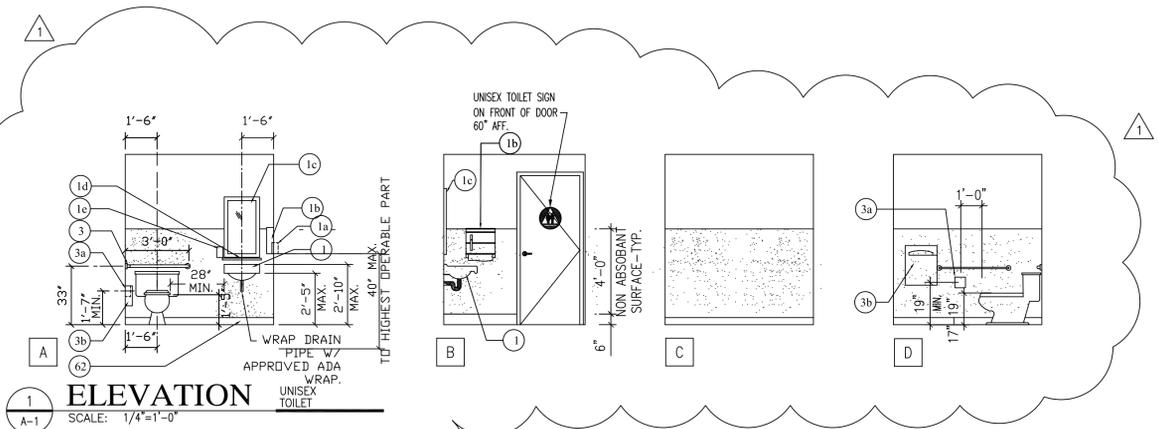
Date

Owner signature

End of Conditions



FIRST FLOOR PLAN
AREA = 2,641 SF



ELEVATION
UNISEX TOILET
SCALE: 1/4" = 1'-0"

- 1 LAVATORY - ACCESSIBLE WALL MOUNTED
- 1a FACIAL TISSUE DISPENSER
- 1b PAPER TOWEL DISPENSER
- 1c 16"Wx 30"H FIXED TILT MIRROR
- 1d SHELF - CHROME
- 1e SOAP DISPENSER
- 2 TOILET SEAT COVER DISPENSER
- 3 TOILET GRAB BAR
- 3a TOILET PAPER DISPENSER
- 3b SANITARY NAPKIN DISPOSAL

<p> INDICATES ROOMS DESIGNED TO COMPLY WITH ADA & TITLE 24, INCLUDING COMPLYING HYGIENE FACILITIES.</p> <p> INDICATES ROOMS WITH HEARING IMPAIRED DEVICES, INCLUDING ALARM & VISUAL NOTIFICATION</p> <p>COORDINATION OF ELECTRICAL DEVICES:</p> <p>THE LOCATION OF WALL MOUNTED ELECTRICAL DEVICES INCLUDING RECEPTACLES, SWITCHES, LIGHT FIXTURES, DATA, CABLE, AND TELEPHONE BOXES SHALL BE COORDINATED TO ACCOMMODATE THE FURNITURE MIRRORS AND LIGHTS TO BE IN COMPLIANCE WITH APPROPRIATE CODES. DEVICE BOXES SHALL NOT BE LOCATED BACK TO BACK IN THE SAME STUD CAVITY.</p>	<p>NOTE:</p> <ol style="list-style-type: none"> FOR GUEST ROOM LAYOUTS AND DIMENSIONS, REFER TO UNIT PLANS ON SHEETS A-8a THROUGH A-8d. REFER TO SHEET A-7 FOR WALL TYPE DESCRIPTIONS. F.E.C. INDICATES THE LOCATION OF FIRE RATED SEMI-RECESSED FIRE EXTINGUISHER CABINETS. COORDINATE LOCATIONS WITH PLUMBING RISERS PRIOR TO FRAMING. ALL DIMENSIONS ARE FROM FACE OF STUD OR FACE OF MASONRY UNLESS NOTED OTHERWISE. PROVIDE 1'x1'x FULL HEIGHT MECHANICALLY FASTENED PLASTIC CORNER GUARDS AT ALL EXPOSED CORNERS. VENDING AREA IS REQUIRED AS SHOWN ON PLANS SEE STRUCTURAL DRAWINGS FOR SHEAR WALL LOCATIONS. IN SHEAR WALL LOCATIONS, PLYWOOD / OSB WILL REPLACE THE SOUND BOARD SPECIFIED. REFER TO SHEET A6.1 FOR DOOR AND WINDOW SCHEDULES. 	<p>GUEST ROOM SUMMARY</p> <p>FIRST FLOOR:</p> <table border="0"> <tr><td>SINGLE (UNIT 'A')</td><td>7</td></tr> <tr><td>DOUBLE (UNIT 'B')</td><td>0</td></tr> <tr><td>SUITE (UNIT 'C')</td><td>0</td></tr> <tr><td>ACCESSIBLE SINGLE (UNIT 'D')</td><td>0</td></tr> <tr><td>TOTAL</td><td>7</td></tr> </table> <p>TOTALS:</p> <table border="0"> <tr><td>SINGLE (UNIT 'A')</td><td>9</td></tr> <tr><td>DOUBLE (UNIT 'B')</td><td>8</td></tr> <tr><td>SUITE (UNIT 'C')</td><td>4</td></tr> <tr><td>ACCESSIBLE SINGLE (UNIT 'D')</td><td>2</td></tr> <tr><td>TOTAL</td><td>23</td></tr> </table> <p>TOTAL ADA ROOMS REQUIRED - PER CODE: 1 TOTAL ADA ROOMS PROVIDED: 2 TOTAL HI ROOMS REQUIRED - PER CODE: 1 TOTAL HEARING IMPAIRED ROOMS PROVIDED: 1</p>	SINGLE (UNIT 'A')	7	DOUBLE (UNIT 'B')	0	SUITE (UNIT 'C')	0	ACCESSIBLE SINGLE (UNIT 'D')	0	TOTAL	7	SINGLE (UNIT 'A')	9	DOUBLE (UNIT 'B')	8	SUITE (UNIT 'C')	4	ACCESSIBLE SINGLE (UNIT 'D')	2	TOTAL	23
SINGLE (UNIT 'A')	7																					
DOUBLE (UNIT 'B')	0																					
SUITE (UNIT 'C')	0																					
ACCESSIBLE SINGLE (UNIT 'D')	0																					
TOTAL	7																					
SINGLE (UNIT 'A')	9																					
DOUBLE (UNIT 'B')	8																					
SUITE (UNIT 'C')	4																					
ACCESSIBLE SINGLE (UNIT 'D')	2																					
TOTAL	23																					

Architect
SHK and Associates
P. O. Box: 7372
Fremont, CA 94537
Tel: (510) 796-9712, Fax: (510) 740-3565



Project
PROPOSED MOTEL

Owner
PRAKASH PATEL & HITESH PATEL
3309 LAVIANA STREET
TUSTIN, CA 92782

Project location
24850 REDLANDS BLVD.
LOMA LINDA, CA. 92395
APN: 0281-091-41

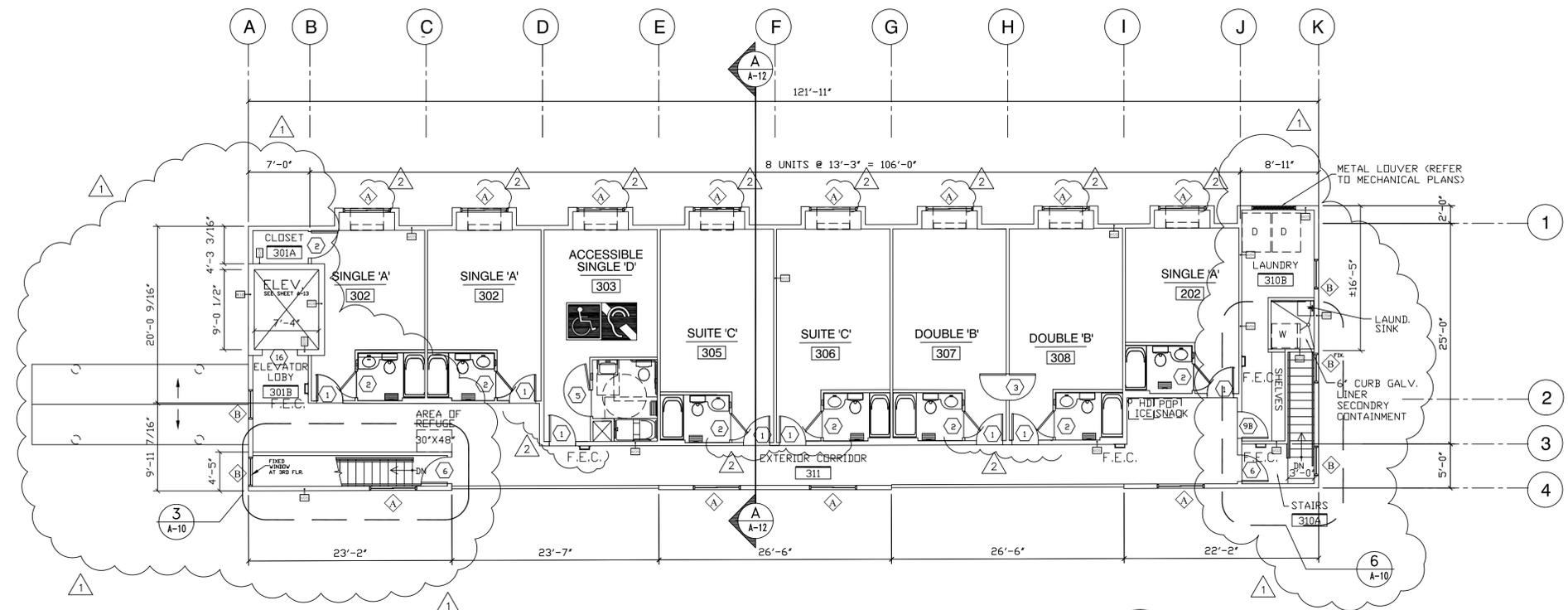
REVISIONS	
NO	DATE
1	PLAN CHECK COMMENTS
2	PLAN CHECK COMMENTS

DRAWINGS ISSUED FOR:
BUILDING PERMIT

08-17-09

FIRST FLOOR PLAN

PROJ. NO.:
09-0215
DATE:
02/15/09
SHEET NUMBER



THIRD FLOOR PLAN
 AREA = 3,140 SF
 SCALE 1/8" = 1'-0"

INDICATES ROOMS DESIGNED TO COMPLY WITH ADA & TITLE 24, INCLUDING COMPLYING HYGIENE FACILITIES. INDICATES ROOMS WITH HEARING IMPAIRED DEVICES, INCLUDING ALARM & VISUAL NOTIFICATION.	NOTE: 1. FOR GUEST ROOM LAYOUTS AND DIMENSIONS, REFER TO UNIT PLANS ON SHEETS A-8a THROUGH A-8d. 2. REFER TO SHEET A-7 FOR WALL TYPE DESCRIPTIONS. 3. F.E.C. INDICATES THE LOCATION OF FIRE RATED SEMI-RECESSED FIRE EXTINGUISHER CABINETS. COORDINATE LOCATIONS WITH PLUMBING RISERS PRIOR TO FRAMING. 4. ALL DIMENSIONS ARE FROM FACE OF STUD OR FACE OF MASONRY UNLESS NOTED OTHERWISE. 5. PROVIDE 1'x1'x FULL HEIGHT MECHANICALLY FASTENED PLASTIC CORNER GUARDS AT ALL EXPOSED CORNERS. 6. VENDING AREA IS REQUIRED AS SHOWN ON PLANS. 7. SEE STRUCTURAL DRAWINGS FOR SHEAR WALL LOCATIONS. IN SHEAR WALL LOCATIONS, PLYWOOD / OSB WILL REPLACE THE SOUND BOARD SPECIFIED. 8. REFER TO SHEET A6.1 FOR DOOR AND WINDOW SCHEDULES.	GUEST ROOM SUMMARY SECOND FLOOR: SINGLE (UNIT 'A') 1 DOUBLE (UNIT 'B') 4 SUITE (UNIT 'C') 2 ACCESSIBLE SINGLE (UNIT 'D') 1 TOTAL 8 TOTALS: SINGLE (UNIT 'A') 9 DOUBLE (UNIT 'B') 8 SUITE (UNIT 'C') 4 ACCESSIBLE SINGLE (UNIT 'D') 2 TOTAL 23 TOTAL ADA ROOMS REQUIRED - PER CODE: 1 TOTAL ADA ROOMS PROVIDED: 2 TOTAL HI. ROOMS REQUIRED - PER CODE: 1 TOTAL HEARING IMPAIRED ROOMS PROVIDED: 1
		COORDINATION OF ELECTRICAL DEVICES: THE LOCATION OF WALL MOUNTED ELECTRICAL DEVICES INCLUDING RECEPTACLES, SWITCHES, LIGHT FIXTURES, DATA, CABLE, AND TELEPHONE BOXES SHALL BE COORDINATED TO ACCOMMODATE THE FURNITURE MIRRORS AND LIGHTS TO BE IN COMPLIANCE WITH APPROPRIATE CODES. DEVICE BOXES SHALL NOT BE LOCATED BACK TO BACK IN THE SAME STUD CAVITY.

Architect
SHK and Associates
 P. O. Box: 7372
 Fremont, CA 94537
 Tel: (510) 796-9712, Fax: (510) 740-3565



Project
PROPOSED MOTEL

Owner
 PRAKASH PATEL & HITESH PATEL
 3309 LAVIANA STREET
 TUSTIN, CA 92782

Project location
 24850 REDLANDS BLVD.
 LOMA LINDA, CA. 92395
 APN: 0281-091-41

REVISIONS

NO	DATE	COMMENTS
1		PLAN CHECK COMMENTS

DRAWINGS ISSUED FOR:
 BUILDING PERMIT

08-17-09

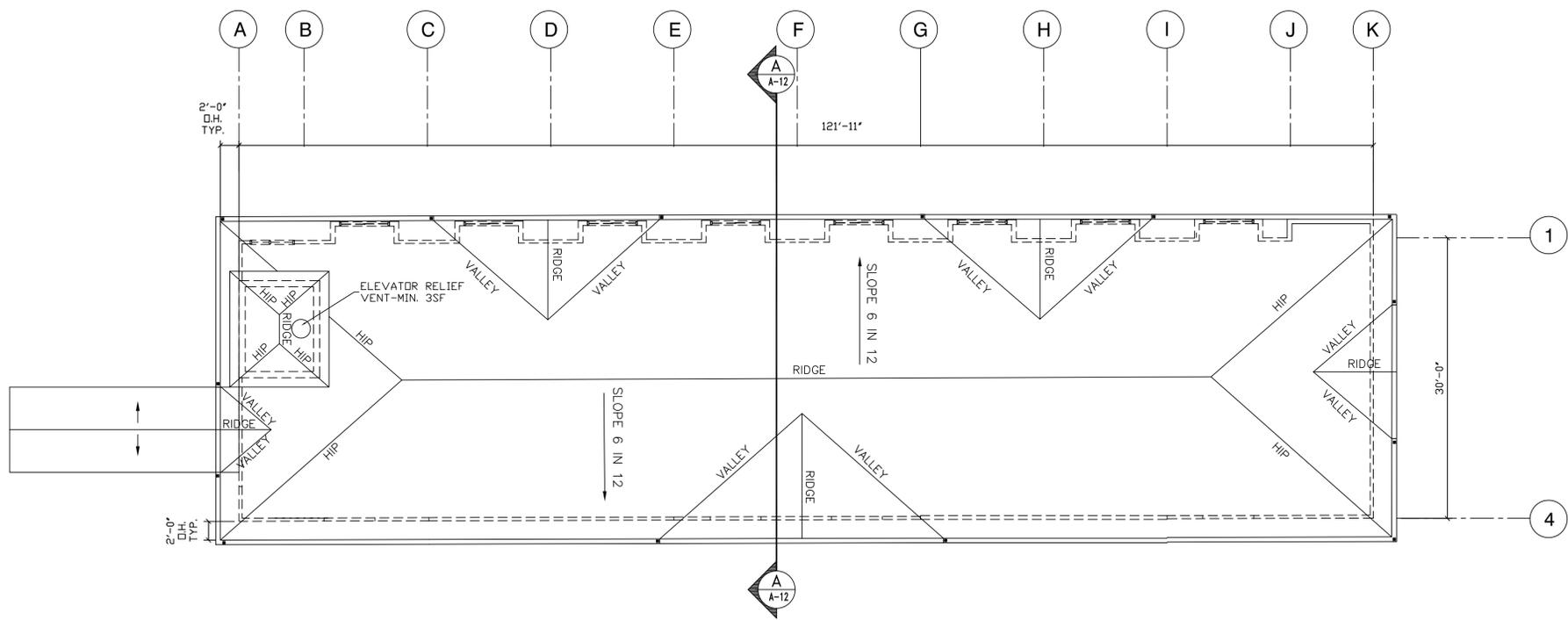
THIRD FLOOR PLAN

PROJ. NO.:
 09-0215

DATE:
 02/15/09

SHEET NUMBER

A-3



ROOF PLAN
SCALE
1/8" = 1' - 0"

Architect
SHK and Associates
 P. O. Box: 7372
 Fremont, CA 94537
 Tel: (510) 796-9712, Fax: (510) 740-3565



Project
PROPOSED MOTEL

Owner
 PRAKASH PATEL & HITESH PATEL
 3309 LAVIANA STREET
 TUSTIN, CA 92782

Project location
 24850 REDLANDS BLVD.
 LOMA LINDA, CA, 92395
 APN: 0281-091-41

REVISIONS

NO	DATE

DRAWINGS ISSUED FOR:
 BUILDING PERMIT

08-17-10

ROOF PLAN

PROJ. NO.:
 09-0215

DATE:
 02/15/09

SHEET NUMBER

A-4



ELEVATION ALONG FREEWAY

3



ELEVATION ALONG WEST

4

General Notes

Owner's Name and Address

No.	Revision/Issue	Date

Sheet Title

ELEVATIONS

Owner's Name and Address

ARCHITECT
SHK AND ASSOCIATES
 P.O. BOX: 7372
 FREMONT, CA 94537
 TEL/FAX: (510) 756-9712



Project Name and Address

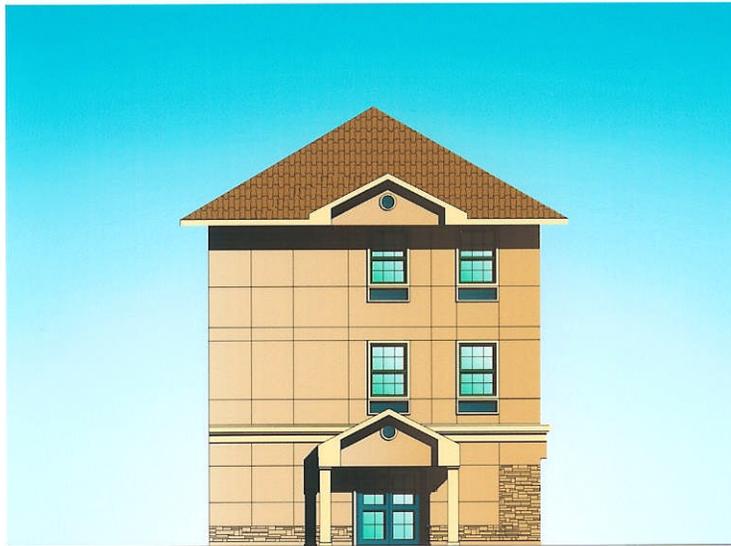
PROPOSED MOTEL
 REDLANDS BLVD.
 LOMA LINDA, CA 92395

Project: 030108
 Date: MARCH, 2006
 Scale: AS SHOWN

Sheet: **A201**

PRELIMINARY

SHK AND ASSOCIATES ARCHITECTS 2007. ALL RIGHTS RESERVED



ELEVATION ALONG REDLAND BLVD ①



ELEVATION ALONG PARKING LOT ②

General Notes

Owner's Name and Address

No.	Revision/Issue	Date

Sheet Title

ELEVATIONS

Architect's Name and Address

SHK AND ASSOCIATES
 ARCHITECT
 P.O. BOX: 7372
 FREMONT, CA 94537
 TEL./FAX: (510) 796-9712



Project Name and Address

PROPOSED MOTEL
 REDLAND BLVD.
 LOMA LINDA, CA 92695

Project	000106	Sheet	A201
Date	MARCH, 2008		
Scale	AS SHOWN		

PRELIMINARY

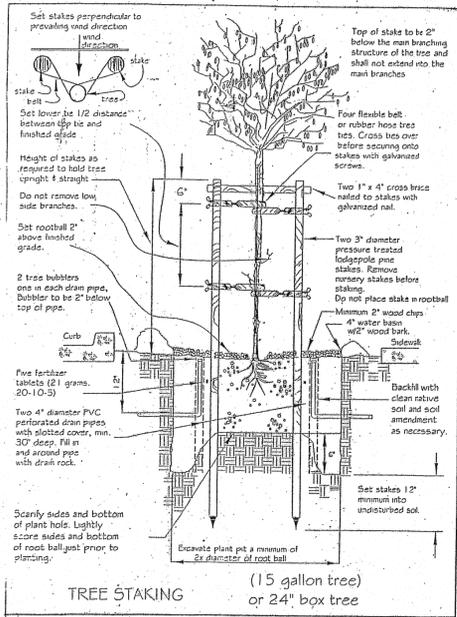
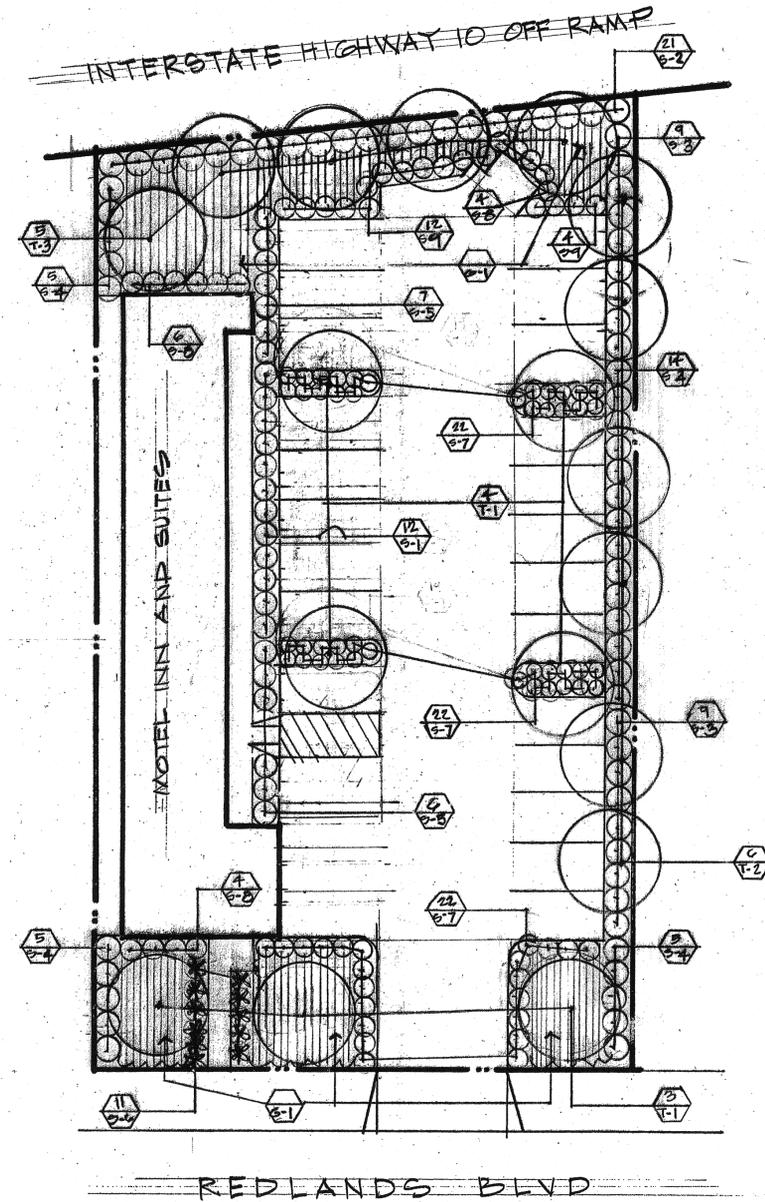
SHK AND ASSOCIATES ARCHITECTS 2007 ALL RIGHTS RESERVED

PLANT LIST

KEY	QUANTITY	BOTANICAL AND COMMON NAME	SIZE
TREES:			
T-1	7	PISTACIA CHINENSIS - CHINESE PISTACHE	126C
T-2	6	PLATANUS WRIGHTII - ARIZONA SYCAMORE	156C
T-3	5	QUERCUS EMORYI - EMORY OAK	186C
SHRUBS:			
S-1	12	CISTUS SALVIFOLIUS - SAGELEAF ROCKROSE	56C
S-2	21	DODONAEA VISCOSEA PURPUREA - PURPLE HOP SHRUB	56C
S-3	18	LEPTOSPERMUM SCOPARIUM 'RUBY GLOW' - AUSTRALIAN TEA TREE	56C
S-4	29	PHOTINIA FRASERII	56C
S-5	13	PUNICA GRANATUM 'CHICO' - DWARF POMEGRANATE	56C
S-6	11	PHORMIUM TENAX 'BRONZE BABY' - NEW ZEALAND FLAX	56C
S-7	66	RAPHIDOLEPIS INDICA 'BALLERINA' - PINK INDIA HAWTHORN	56C
S-8	14	RHUS OVATA - SUGAR BUSH	56C
S-9	16	XYLOSMA CONGESTUM - SHINY XYLOSMA	56C
GROUND COVERS:			
G-1	AS REQ'D	SOXYA HETEROPHYLLA - AUSTRALIAN BLUEBELL CREEPER	16C

LEGEND

 SOXYA AREAS 3' O.C.



REVISIONS	BY
PLANNING PLAN REVISED 9/16/09	WTS

PLANNING PLAN
MOTEL INN AND SUITES
 REDLANDS BLVD
 LOMA LINDA, CALIFORNIA



WESLEY T. SAKAMOTO
LANDSCAPE ARCHITECT #016
 665 HOLYHOOK DRIVE
 SAN LEANDRO, CA 94578-1101



DRAWN	WTS
CHECKED	
DATE	12/10/07
SCALE	1/8" = 1'-0"
JOB NO.	
SHEET	
L1	
OF	SHEETS